

# Lindsay City Council Agenda

## Regular Meeting

Council Chambers at City Hall  
251 E. Honolulu, Lindsay, California  
Tuesday, July 14, 2015  
6:00PM

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1. a) Call to Order: 6:00 p.m.  
b) Roll Call: Council members Salinas, Mecum, Kimball, Mayor Pro Tem Sanchez, Mayor Padilla.  
c) Flag Salute: Mayor Pro Tem SANCHEZ.

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  2. Public Comment: The public is invited to comment on any subject under the jurisdiction of the Lindsay City Council, including agenda items, other than noticed public hearings. Comments shall be limited to (3) minutes per person, with 30 minutes overall for the entire comment period, unless otherwise indicated by the Mayor.

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  3. Consent Calendar: These items are considered routine and will be enacted by one motion, unless separate discussion is requested by Council or members of the public.  
**Request for approval of the following:** (pg. 1-17)
    - a) Meeting Minutes for June 23<sup>rd</sup> & July 6<sup>th</sup>, 2015.
    - b) Warrant Register for June 19<sup>th</sup> & 30<sup>th</sup> and July 1<sup>st</sup>, 2015.
    - c) Accept Treasurer's Report for June 30, 2015.
    - d) Accept the 2015-2 Waste Water Treatment Plant Headwork's Renovation Project as complete & direct the City Clerk to File a Notice of Completion.

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  4. DISCUSSION AND POSSIBLE APPOINTMENT & APPROVAL OF INTERIM CITY MANAGER AND EMPLOYMENT AGREEMENT. (pg. 18-21)  
Presented by City Attorney.

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  5. PUBLIC HEARING: CONSIDERATION OF RESOLUTION 15-34 APPROVING THE MODIFICATION OF PARCEL MAP FOR APN's 199-280-001 & 199-280-002. (pg. 22-38)  
Presented by Bill Zigler, Planning & Economic Development Director.

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  6. CONTINUED PUBLIC HEARING: CONSIDERATION OF RESOLUTION 15-12 APPROVING SITE PLAN REVIEW 15-03 AND ACCEPTING A MITIGATED NEGATIVE DECLARATION. (pg. 39-119)  
Presented by Bill Zigler, Planning & Economic Development Director.

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  7. PUBLIC HEARING: CONSIDERATION OF RESOLUTION 15-37 APPROVING THE ORDERING THE CONTINUED MAINTENANCE OF LANDSCAPE AND LIGHTING MAINTENANCE DISTRICTS & CONFIRMING THE ENGINEER'S REPORT AND ASSESSMENT FOR FISCAL YEAR 2015-16. (pg. 120-123)  
Presented by Mike Camarena, City Services Director.

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  8. CONSIDERATION OF RESOLUTION 15-36 APPROVING SITE PLAN REVIEW 15-05 FOR PROPERTY DEVELOPMENT ON THE SOUTHEAST CORNER OF WESTWOOD AVENUE & HERMOSA ST (APN: 205-040-005). (pg. 124-142)  
Presented by Bill Zigler, Planning & Economic Development Director.

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  9. COUNCIL REPORTS.  
Presented by Council members.

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  10. STAFF REPORTS.  
Presented by The Acting & Interim City Managers & Staff.

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  11. ADJOURN. The next Regular meeting is scheduled for **TUESDAY, JULY 28, 2015 at 6:00 PM** in the Council Chambers at City Hall, 251 E. Honolulu, Lindsay, CA 93247.
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Materials related to an Agenda item submitted to the legislative body after distribution of the Agenda Packet are available for public inspection in the office of the City Clerk during normal business hours. Complete agenda is available at [www.lindsay.ca.us](http://www.lindsay.ca.us) In compliance with the Americans with Disabilities Act & Ralph M. Brown Act, if you need special assistance to participate in this meeting, or to be able to access this agenda and documents in the agenda packet, please contact the office of the City Clerk at (559) 562-7102 ext 8031. Notification 48 hours prior to the meeting will enable the City to ensure accessibility to this meeting and/or provision of an alternative format of the agenda and documents in the agenda packet.

# Lindsay City Council Meeting Minutes

Pg. 8061

Regular Meeting  
Council Chambers at City Hall  
251 East Honolulu, Lindsay, California  
Tuesday, June 23, 2015  
6:00PM

## CALL TO ORDER.

Mayor PADILLA called the Meeting of the Lindsay City Council to order at 6:00 p.m. in the Council Chambers at City Hall, 251 E. Honolulu, Lindsay, and California.

**COUNCIL PRESENT:** SALINAS, MECUM, KIMBALL, Mayor Pro Tem SANCHEZ, Mayor PADILLA.

**COUNCIL ABSENT:** None.

**FLAG SALUTE:** Council member KIMBALL.

## **PUBLIC COMMENT:**

Eric Sinclair-commented on top priorities for the City of Lindsay  
Delma Mecum-urged Council members to reconsider raising utility rates  
Ellen Blumer-request to consider including prayer at Council meetings  
Lorena Vasquez-disagrees with raising utility rates, consider other options  
Tony Kellog- against raising utility rates,  
Rene Salazar-provided suggestions to address the drought, shrink landscape area and market our city to tourists.  
Brian Watson-citizen concerns regarding the appearance of impropriety by Council members and retain Acting City Manager & Acting Police Chief to minimize impact on the budget.  
Yolanda Flores-commented on previous inaction of Grand Jury and Dept of Justice  
Eduardo Cardenas-raising utility rates would have a negative impact on local businesses.

## **CONSENT CALENDAR:**

### **Request for approval of the following:**

- a) Meeting Minutes for June 9<sup>th</sup>, 2015.
- b) Warrant Register for June 12<sup>th</sup>, 2015.
- c) Accept revised Thunderbolt Agreement for FY2015/16.
- d) Temporary Use Permit for Shakespeare in the Plaza August 8<sup>th</sup>, 9<sup>th</sup>, 13<sup>th</sup> & 15<sup>th</sup>.

## **ACTION:**

On Motion by MECUM and Second by SANCHEZ, the Lindsay City Council approved the Consent Calendar via Minute Order.

AYES: MECUM, SANCHEZ, SALINAS, KIMBALL, PADILLA.  
NOES: None.  
ABSENT: None.

## **CONSIDERATION OF RESOLUTION 15-31 DECLARING THE INTENT & COMMITMENT TO USE SILVERCREST VILLAGE, PHASE I, OUTLOT "A" AS PART OF THE OLIVE BOWL/KAKU PARK COMPLEX.**

Planning & Economic Development Director, Bill Zigler introduced this item and staff report. He stated staff respectfully requests to designate Silvercrest Village Phase I Outlot "A" for park use via the attached resolution. The Department of Finance requires a resolution from the City indicating the commitment and intent of the City to use the property as part of the park.

He explained that Outlot "A" was acquired by the City of Lindsay as a part of the Silvercrest Village subdivision. It was transferred to the RDA in May of 2008 to facilitate the Olivebowl Park housing project. That project never came to fruition and the property remained with the RDA until the dissolution of the RDA, at which time the property was transferred to the Successor Agency to the RDA. The Successor Agency and the Oversight Board approved the transfer of Outlot "A" to the City of Lindsay for government use via resolutions SA15-02 and OB15-03, which are also included in your packet

So the area being discussed is a long narrow strip of land approximately 1.35 acres. If we are able to retain this area it would expand the park area. We were not sure what the state was going to let us keep or not keep. The

**RESOLUTION 15-31 DECLARING THE INTENT & COMMITMENT TO USE SILVERCREST VILLAGE, PHASE I, OUTLOT "A" AS PART OF THE OLIVE BOWL/KAKU PARK COMPLEX cont.**

state wants to see most Redevelopment Agencies properties sold, with the proceeds being distributed to taxing entities. We made an argument for Olive Bowl/Kaku Park and it was readily accepted but they wanted to see commitment from Council that this would be dedicated for park use.

Staff recommends Council approval of the attached resolution in order to preserve Outlot "A" for park use.

With no questions from Council Mayor PADILLA asked what action Council would like to take on this item.

**ACTION:**

On Motion by SALINAS and Second by KIMBALL, the Lindsay City Council approved Resolution 15-31 DECLARING THE INTENT AND COMMITMENT TO USE SILVERCREST VILLAGE, PHASE I, OUTLOT "A" AS PART OF THE OLIVE BOWL/KAKU PARK COMPLEX.

AYES: SALINAS, KIMBALL, MECUM, SANCHEZ PADILLA.  
NOES: None.  
ABSENT: None

**ACTIONS ON LIGHTING & LANDSCAPE MAINTENANCE ASSESSMENT DISTRICTS.**

- a) Res. 15-32 giving Preliminary Approval of the Engineer's Report for the FY 2015-2016 Landscape & Lighting Maintenance Districts.
- b) Res. 15-33 Declaring the Intent to Levy & Collect Assessments for FY 2015-2016 and Setting a Public Hearing for July 14, 2015

City Services Director, Mike Camarena introduced this item and staff report. He stated every year, the Engineer of Work is ordered to prepare the report for the upcoming Fiscal Year in accordance with Article 4, Division 15, of the Streets and Highways Code, "Landscaping and Lighting Act of 1972" of the State of California.

The Engineer's report outlines the budgeted expenses for the present fiscal year, the actual expenses through May, and the projected expenses for the upcoming fiscal year. Each year there are adjustments made due to increases and/or actual expenses.

Attached in the Council packet are the engineer's reports for the Assessment Districts. The reports identify the cost for maintenance and administration of the districts for the 2015-2016 fiscal year based on 2014-2015 expenses. It is projected that all monies will be expended in these funds by the end of the fiscal year.

Summarized in the agenda are the (8) Assessment Districts and what the impacts of the proposed budget adjustment are for each district. He summarized each district individually as follows:

- Sierra View Estates Assessment District had a proposed decrease of \$3.21 month per lot
- Heritage Park Assessment District had a proposed decrease of \$1.52 per month per lot
- Parkside Estates Assessment District had a proposed increase of \$0.79 per month per property
- Sweet Brier Plaza (Samoa) Assessment District had a proposed decrease of \$9.01 per month per property
- Sweet Brier Plaza (Hermosa) Assessment District had a proposed decrease of \$9.34 per month per property
- Sierra Vista Assessment District had a proposed increase of \$3.85 per month per property
- Maple Valley Assessment District had a proposed increase of \$0.16 per month per property
- Pelous Ranch Assessment District had a proposed increase of \$1.03 per month per property

Council questions included is water metered specifically for the landscape areas, clarification on charges for district 6 and what are reductions & increases based on and potential conflicts for Council members who live near the districts.

With no further questions from Council, Mayor PADILLA asked what if any action Council would like to take on these resolutions and asked legal counsel if these resolutions can be taken up together.

City Attorney stated they maybe taken up together.

Council members what if any action do you wish to take on Resolution 15-32 giving Preliminary Approval of

**ACTIONS ON LIGHTING & LANDSCAPE MAINTENANCE ASSESSMENT DISTRICTS.**

the Engineer's Report for the FY 2015-2016 Landscape & Lighting Maintenance Districts and Resolution 15-33 Declaring the Intent to Levy & Collect Assessments for FY 2015-2016 and Setting a Public Hearing for the 14<sup>th</sup> day of July 14, 2015.

**ACTION:**

On Motion by SALINAS and Second by MECUM, the Lindsay City Council approved RESOLUTION 15-32 GIVING PRELIMINARY APPROVAL OF THE ENGINEER'S REPORT FOR THE FY 2015/16 LANDSCAPE & LIGHTING DISTRICTS AND RESOLUTION 15-33 DECLARING THE INTENT TO LEVY & COLLECT ASSESSMENTS FOR FY 2015/16 AND SETTING A PUBLIC HEARING FOR JULY 14, 2015.

AYES: SALINAS, MECUM, KIMBALL, SANCHEZ, PADILLA.  
NOES: None.  
ABSENT: None

**STUDY SESSION REGARDING CITY OF LINDSAY MEMBERSHIP IN THE CONSOLIDATED WASTE MANAGEMENT AUTHORITY.**

City Services Director, Mike Camarena introduced this item and staff report. He stated that at the Consolidated Waste Management Authority (CWMA) meeting of June 18, 2015, the Board discussed the Future of the Joint Powers of Authority (JPA) and the role of the new Administrator.

CWMA is asking that each agency present, discuss the following questions to their respective Council and boards.

- Does the City foresee future relationship with the CWMA?
- What type of agency does the City want?
- If Lindsay supports the continuation of the CWMA, are the current administrator job duties adequate? Does the CWMA need to revise this job title to an Executive Director with the associated responsible duties and authority?

The existing JPA agreement has gone through a lengthy review and revision process and is being recommended for approval, provided there is support for the JPA's future.

Some background, the CWMA was formed in 1999 and the City of Lindsay was actually one of the forming members. The intent at that time was to provide a sort of clearing house for recycling which at that time there was a tremendous goal of 25% diversion that the state was mandating each agency to do. It was just an incredible goal at that time. Included in the Council packet are reports of some of the benefits provided to each agency. Examples of those are Battery Collection program, Construction & Demolition Debris Collection Program, Household Hazardous Waste, Per Capita Waste Disposal Tonnage Report, Lindsay's Waste Diversion Trend Report. The administrator tracks legislation that is due to come in to us regarding proposed Recycling, prepares required reports to the State on our behalf

So the purpose of the Study Session tonight is to review if this program is of benefit to us and take it back to CWMA and report to them what Lindsay's position will be.

Council/Staff discussion followed, questions included cost for membership, recycling programs offered to the City of Lindsay, program administration & reporting & addressing the presented questions.

**ACTION:**

Council member consensus was to continue CWMA membership and deferred to Board discussion of Administrator duties.

**COUNCIL REPORTS**

Council member MECUM: Reported Council reconsideration of utility rates/wait on hiring Interim City Manager

Council member KIMBALL: Reported on TCAG /Transit/Transportation dollars & clarified use of Air Quality Funding & Measure "R" Funding

Council member SALINAS: encouraged citizen attendance at budget meetings

**COUNCIL REPORTS continued**

Mayor Pro Tem SANCHEZ: attendance of CWMA meeting, Drug Court Graduation and HOW Dinner (She asked Marie Arroyo to describe the dinner /program) and she would represent citizens at Budget Study Session whether they attend the meetings or not.

Mayor PADILLA: 2015 League of CA Cities Conference, 7<sup>th</sup> Day Adventist School of Theology Graduation, PACE Program for Latina Woman

**STAFF REPORTS**

Acting City Manager Carmen Wilson thanked staff for their continued support and asked for brief departmental reports to Council.

She provided a report from the Wellness Center

City Services Director Mike Camarena reported on the following

Water conservation regulations

Annual Consumer Confidence Report(CCR) will be mailed on 7/1

DBCP Compliance Notices for Well 14 will be mailed out 7/10

Sequoia Ave Pedestrian Project (\$300,000) has begun and could take up to 45 days

Solar Project at WWTP has started

McDermont Solar had to be downsized

CalRecycle funding approved \$16,500 for park playground equipment

Prospective Eagle Scout Project refurbishing the old cannon at the park

Planning / Economic Development Department reported the following:

Cleanup of former Lindsay Foods site on Mirage (proposed new grocery store)

Negotiations ongoing for old Tulare Frozen Foods Site

Self Help notified staff their project has changed

McDermont Director Clint Ashcraft reported the following:

Facility has seen approximately 15,000 kids this year up about 1500 from last year

Membership is up about p 10% on over last year

Birthday parties and typical activity is also growing

400 LUSD kids attending Summer Camp in the morning

Red Zone attends in the afternoon (HS after school program)

Olympian run Wrestling Camp

Small batting cage will be placed in arcade area

Striving to make Zip line ADA compliant

**EXECUTIVE SESSION**

- 1) Conference with Legal Counsel regarding pending litigation - GC§54956.9(d)(1)

Brian Clower vs. City of Lindsay

Tulare County Superior Court Case #257764

- 2) Public Employee Appointment

Title: Interim City Manager

GC§54957(b)(1)

**Mayor PADILLA reconvened the regular meeting and stated there was nothing to report.**

**ADJOURN.** Upon motion of MECUM and Second of SANCHEZ Mayor PADILLA adjourned the Regular Meeting of the Lindsay City Council at 8:25 pm The next Regular City Council Meeting is scheduled for **TUESDAY, JULY14th, 2015 at 6PM** in the Council Chambers at City Hall, 251 E. Honolulu, Lindsay, CA 93247.

ATTEST:

CITY COUNCIL OF THE CITY OF LINDSAY

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Carmela Wilson, City Clerk

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Ramona Villarreal-Padilla, Mayor

Lindsay City Council Meeting Minutes

Pg. 8065

Special Meeting  
Community Development Conference Room  
251 East Honolulu, Lindsay, California  
Monday, July 6, 2015  
3:30PM

**CALL TO ORDER.**

Mayor PADILLA called the Meeting of the Lindsay City Council to order at 3:30 p.m. in the Community Development Conference Room, 251 E. Honolulu, Lindsay, and California.

**COUNCIL PRESENT:** SALINAS, KIMBALL, Mayor Pro Tem SANCHEZ, Mayor PADILLA.

**COUNCIL ABSENT:** MECUM.

**FLAG SALUTE:** Council member SALINAS.

**PUBLIC COMMENT:** None.

**EXECUTIVE SESSION**

PUBLIC EMPLOYMENT APPOINTMENT- Pursuant to Government Code §54957(b)(1)  
Title: INTERIM CITY MANAGER

Mayor PADILLA reconvened the Special meeting and stated there was nothing to report.

**ADJOURN.** Upon motion of KIMBALL and Second of SANCHEZ Mayor PADILLA adjourned the Special Meeting of the Lindsay City Council at 5:45 pm The next Regular City Council Meeting is scheduled for **TUESDAY, JULY 14th, 2015 at 6PM** in the Council Chambers at City Hall, 251 E. Honolulu, Lindsay, CA 93247.

ATTEST:

CITY COUNCIL OF THE CITY OF LINDSAY

\_\_\_\_\_  
Carmela Wilson, City Clerk

\_\_\_\_\_  
Ramona Villarreal-Padilla, Mayor

**CITY OF LINDSAY ORGANIZATIONAL CHART FOR 2014-2015**

| <b>FUND /DEPT</b>                         | <b>TITLE/DESCRIPTION</b>    |         |                      |
|---|-----------------------------|---------|----------------------|
| 1014010                                   | CITY COUNCIL                |         |                      |
| 1014040                                   | CITY MANAGER                |         |                      |
| 1014050                                   | FINANCE                     |         |                      |
| 1014060                                   | CITY ATTORNEY               |         |                      |
| 1014090                                   | NON-DEPARTMENTAL            |         |                      |
| 1014110                                   | PUBLIC SAFETY               |         |                      |
| 1014120                                   | PUBLIC WORKS DEPT.          |         |                      |
| 1014130                                   | STREETS                     |         |                      |
| 1014210                                   | PARKS                       |         |                      |
| 1024111                                   | ASSET FORFEITURE            |         | RESTRICTED FUND      |
| 2614160                                   | GAS TAX-MAINTENANCE         |         | RESTRICTED FUND      |
| 2634180                                   | TRANSPORTATION              |         | RESTRICTED FUND      |
| 2644190                                   | TRANSIT FUND                |         | RESTRICTED FUND      |
| 3004300                                   | MCDERMONT OPERATION         |         | ENTERPRISE FUND      |
| 4004400                                   | WELLNESS CENTER/AQUATIC     |         | ENTERPRISE FUND      |
| 5524552                                   | WATER                       |         | ENTERPRISE FUND      |
| 5534553                                   | SEWER                       |         | ENTERPRISE FUND      |
| 5544554                                   | REFUSE                      |         | ENTERPRISE FUND      |
| 5564556                                   | LAND APPLICATION            |         | SPECIAL REVENUE FUND |
| 600                                       | CAPITAL IMPROVEMENT FUND    | -       | ISF                  |
| 8414140                                   | CURB & GUTTER               |         | SPECIAL REVENUE FUND |
| 856                                       | STORM DRAIN SYSTEM          |         | SPECIAL REVENUE FUND |
| 857                                       | DOMESTIC WASTEWATER         |         | SPECIAL REVENUE FUND |
| 660                                       | SUCCESSOR AGENCY FUND - RDA |         |                      |
| 662                                       | SUCCESSOR AGENCY FUND - LMI |         |                      |
| <b>ASSESSMENT DISTRICTS:</b>              |                             |         |                      |
| 8834883                                   | SIERRA VIEW                 | 8884888 | PARKSIDE ESTATES     |
| 8844884                                   | HERITAGE PARK               | 8894889 | SIERRA VISTA         |
| 8854885                                   | INGOLDSBY                   | 8904890 | MAPLE VALLEY         |
| 8864886                                   | SAMOA STREET                | 8914891 | PELOUS RANCH         |
| 8874887                                   | SWEETBRIER UNITS            |         |                      |
| <b>HOUSING AND COMMUNITY DEVELOPMENT:</b> |                             |         |                      |
| 7004700                                   | CDBG REVOLVING LN FUND      |         |                      |
| 7204720                                   | HOME REVOLVING LN FUND      |         |                      |
| 779                                       | IMPOUND ACCOUNT             |         |                      |

**NOTE: All payments using the object code of 200: EXAMPLE XXX-200-XXX are Liability accounts for monies collected from other sources - i.e. payroll deductions, deposits, impounds, etc - and are not Expenses to City**

| CHECK # | DATE     | VENDOR # | VENDOR                         | FUND    | ACCOUNT | AMOUNT      |
|---------|----------|----------|--------------------------------|---------|---------|-------------|
| 83993   | 06/30/15 | 5781     | ACE HEATING & AIR CONDITIONING | 3004775 | 064000  | \$4,994.60  |
| 83993   | 06/30/15 | 5781     | ACE HEATING & AIR CONDITIONING | 3004775 | 064000  | \$6,430.75  |
| 83994   | 06/30/15 | 4861     | ACOR PRIVATE SECURITY          | 4004400 | 069115  | \$170.00    |
| 83994   | 06/30/15 | 4861     | ACOR PRIVATE SECURITY          | 4004400 | 069115  | \$85.00     |
| 83994   | 06/30/15 | 4861     | ACOR PRIVATE SECURITY          | 4004400 | 069115  | \$306.00    |
| 83928   | 06/19/15 | 2873     | ADVANTAGE ANSWERING            | 5524552 | 033001  | \$72.48     |
| 83928   | 06/19/15 | 2873     | ADVANTAGE ANSWERING            | 5534553 | 033001  | \$72.47     |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$8.68      |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$13.18     |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$11.88     |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014130 | 022000  | \$22.14     |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$30.26     |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$65.16     |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$110.61    |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$122.06    |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$123.34    |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$171.45    |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 1014120 | 022000  | \$179.41    |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 5524552 | 022000  | \$7.15      |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 5524552 | 022000  | \$36.94     |
| 83995   | 06/30/15 | 007      | AG IRRIGATION SALES            | 5524552 | 022000  | \$25.05     |
| 83996   | 06/30/15 | 4876     | AGUIRRE PRINTING & EMBROIDERY  | 3004300 | 069102  | \$161.14    |
| 83997   | 06/30/15 | 4908     | AMERITAS LIFE INSURANCE        | 101     | 200260  | \$973.00    |
| 83997   | 06/30/15 | 4908     | AMERITAS LIFE INSURANCE        | 101     | 200260  | \$2,775.16  |
| 83997   | 06/30/15 | 4908     | AMERITAS LIFE INSURANCE        | 101     | 200260  | \$200.00    |
| 83998   | 06/30/15 | 4924     | ASI ADMINISTRATIVE SOLUTIONS   | 1014090 | 015010  | \$72.60     |
| 83999   | 06/30/15 | 3428     | AT&T MOBILITY                  | 1014110 | 037000  | \$68.26     |
| 83999   | 06/30/15 | 3428     | AT&T MOBILITY                  | 1014110 | 037000  | \$72.35     |
| 83999   | 06/30/15 | 3428     | AT&T MOBILITY                  | 3004300 | 069172  | \$10.44     |
| 84000   | 06/30/15 | 5594     | BACKFLOW APPARATUS             | 5524552 | 022000  | \$95.90     |
| 84001   | 06/30/15 | 3966     | BEATWEAR INC.                  | 3004300 | 055019  | \$243.56    |
| 83929   | 06/19/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$413.32    |
| 83929   | 06/19/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$237.88    |
| 83929   | 06/19/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$1,573.06  |
| 83929   | 06/19/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$197.59    |
| 83929   | 06/19/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$60.51     |
| 83929   | 06/19/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$26.08     |
| 83929   | 06/19/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$26.55     |
| 84002   | 06/30/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$239.97    |
| 84002   | 06/30/15 | 3797     | BETSON IMPERIAL PARTS          | 3004300 | 069092  | \$87.45     |
| 84003   | 06/30/15 | 2047     | BLUE SHIELD                    | 101     | 200260  | \$3,854.09  |
| 84003   | 06/30/15 | 2047     | BLUE SHIELD                    | 101     | 200260  | \$9,085.96  |
| 84003   | 06/30/15 | 2047     | BLUE SHIELD                    | 101     | 200260  | \$47,433.45 |
| 83930   | 06/19/15 | 051      | BSK                            | 5524552 | 022001  | \$226.00    |
| 83930   | 06/19/15 | 051      | BSK                            | 5524552 | 022001  | \$120.00    |
| 83930   | 06/19/15 | 051      | BSK                            | 5524552 | 022001  | \$30.00     |
| 84004   | 06/30/15 | 051      | BSK                            | 5524552 | 022001  | \$295.00    |
| 84004   | 06/30/15 | 051      | BSK                            | 5524552 | 022001  | \$295.00    |

| CHECK # | DATE     | VENDOR # | VENDOR                    | FUND    | ACCOUNT | AMOUNT      |
|---------|----------|----------|---------------------------|---------|---------|-------------|
| 84004   | 06/30/15 | 051      | BSK                       | 5524552 | 022001  | \$295.00    |
| 84004   | 06/30/15 | 051      | BSK                       | 5524552 | 022001  | \$195.00    |
| 84004   | 06/30/15 | 051      | BSK                       | 5524552 | 022001  | \$30.00     |
| 84004   | 06/30/15 | 051      | BSK                       | 5524552 | 022001  | \$30.00     |
| 84004   | 06/30/15 | 051      | BSK                       | 5524552 | 022001  | \$120.00    |
| 84004   | 06/30/15 | 051      | BSK                       | 5524552 | 022001  | \$120.00    |
| 84005   | 06/30/15 | 1690     | CDW-G                     | 4004400 | 022000  | \$1,047.57  |
| 83931   | 06/19/15 | 4203     | CEMEX, INC                | 2614160 | 065013  | \$414.50    |
| 83931   | 06/19/15 | 4203     | CEMEX, INC                | 2614160 | 065013  | \$631.85    |
| 83931   | 06/19/15 | 4203     | CEMEX, INC                | 2614160 | 065013  | \$638.82    |
| 84006   | 06/30/15 | 4203     | CEMEX, INC                | 2614160 | 065013  | \$316.47    |
| 83932   | 06/19/15 | 5796     | CENTRAL VALLEY PRESSORT   | 5524552 | 031000  | \$868.00    |
| 84078   | 7/1/15   | 5796     | CENTRAL VALLEY PRESORT    | 5524552 | 31000   | \$3,000.00  |
| 84007   | 06/30/15 | 5277     | CHARLES SOSA              | 1014110 | 022015  | \$1,000.00  |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 1014120 | 022000  | \$218.39    |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 1014130 | 022000  | \$218.39    |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 1014210 | 022000  | \$218.39    |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 3004300 | 069091  | \$538.60    |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 3004300 | 069102  | \$21.57     |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 5524552 | 022000  | \$218.39    |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 5534553 | 022000  | \$218.39    |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 5544554 | 022000  | \$218.39    |
| 83933   | 06/19/15 | 5832     | CINTAS CORPORATION        | 5564556 | 022000  | \$218.39    |
| 84008   | 06/30/15 | 5832     | CINTAS CORPORATION        | 3004300 | 069091  | \$596.21    |
| 84008   | 06/30/15 | 5832     | CINTAS CORPORATION        | 3004300 | 069091  | \$647.44    |
| 84008   | 06/30/15 | 5832     | CINTAS CORPORATION        | 3004300 | 069102  | \$21.57     |
| 84008   | 06/30/15 | 5832     | CINTAS CORPORATION        | 3004300 | 069102  | \$21.61     |
| 83950   | 06/19/15 | 279      | CITY OF PORTERVILLE       | 1014110 | 031010  | \$804.00    |
| 83950   | 06/19/15 | 279      | CITY OF PORTERVILLE       | 1014110 | 022015  | \$9,000.00  |
| 84043   | 06/30/15 | 279      | CITY OF PORTERVILLE       | 5534553 | 031007  | \$1,032.00  |
| 84009   | 06/30/15 | 5920     | COMMERCIAL FITNESS        | 3004775 | 064000  | \$63,714.80 |
| 84009   | 06/30/15 | 5920     | COMMERCIAL FITNESS        | 3004775 | 064000  | \$5,459.76  |
| 83934   | 06/19/15 | 2319     | COMPUTER SYSTEMS PLUS     | 1014050 | 036008  | \$45.00     |
| 83935   | 06/19/15 | 4567     | COUNTY OF TULARE IT RADIO | 1014110 | 031005  | \$97.50     |
| 83936   | 06/19/15 | 102      | CULLIGAN                  | 5524552 | 022000  | \$238.20    |
| 83936   | 06/19/15 | 102      | CULLIGAN                  | 5524552 | 022000  | \$642.90    |
| 83936   | 06/19/15 | 102      | CULLIGAN                  | 5524552 | 022000  | \$317.96    |
| 83936   | 06/19/15 | 102      | CULLIGAN                  | 5524552 | 022000  | -\$16.07    |
| 83936   | 06/19/15 | 102      | CULLIGAN                  | 5524552 | 022000  | -\$15.50    |
| 83936   | 06/19/15 | 102      | CULLIGAN                  | 5534553 | 019000  | \$151.46    |
| 83937   | 06/19/15 | 316      | DEPT OF JUSTICE           | 1014110 | 066007  | \$35.00     |
| 83937   | 06/19/15 | 316      | DEPT OF JUSTICE           | 1014110 | 039001  | \$885.00    |
| 84010   | 06/30/15 | 5599     | DEROSA SALES              | 3004300 | 069116  | \$193.44    |
| 84011   | 06/30/15 | 2223     | DIANE BUCAROFF            | 1014070 | 031000  | \$330.00    |
| 84012   | 06/30/15 | 5322     | DISH NETWORK              | 4004400 | 033001  | \$157.69    |
| 83938   | 06/19/15 | 119      | DOUG DELEO WELDING        | 1014130 | 022015  | \$7.45      |
| 84013   | 06/30/15 | 119      | DOUG DELEO WELDING        | 1014120 | 022000  | \$43.20     |
| 84013   | 06/30/15 | 119      | DOUG DELEO WELDING        | 1014120 | 022000  | \$49.46     |

| CHECK # | DATE     | VENDOR # | VENDOR                    | FUND    | ACCOUNT | AMOUNT     |
|---------|----------|----------|---------------------------|---------|---------|------------|
| 84013   | 06/30/15 | 119      | DOUG DELEO WELDING        | 3004300 | 069092  | \$60.00    |
| 84013   | 06/30/15 | 119      | DOUG DELEO WELDING        | 3004300 | 069092  | \$60.00    |
| 84013   | 06/30/15 | 119      | DOUG DELEO WELDING        | 5524552 | 023000  | \$221.97   |
| 84014   | 06/30/15 | 3583     | EMBED/ HELIX USA LLC      | 3004300 | 069069  | \$5,100.00 |
| 84015   | 06/30/15 | 4460     | EVANS FEED & LIVESTOCK    | 1014110 | 031010  | \$101.71   |
| 83939   | 06/19/15 | 129      | FEDEX                     | 1014110 | 031000  | \$130.28   |
| 84016   | 06/30/15 | 129      | FEDEX                     | 1014050 | 024000  | \$34.23    |
| 84017   | 06/30/15 | 3808     | FOSTER FARMS DAIRY        | 3004300 | 069116  | \$567.65   |
| 84018   | 06/30/15 | 137      | FRIANT WATER AUTHORITY    | 5524552 | 022010  | \$1,495.00 |
| 84018   | 06/30/15 | 137      | FRIANT WATER AUTHORITY    | 5524552 | 022010  | \$7,758.24 |
| 84018   | 06/30/15 | 137      | FRIANT WATER AUTHORITY    | 5524552 | 022010  | \$11.77    |
| 84019   | 06/30/15 | 4868     | GILTON CONSTRUCTION       | 3004300 | 069092  | \$2,540.00 |
| 84020   | 06/30/15 | 2283     | GOLDEN STATE FLOW         | 5524552 | 022000  | \$1,185.79 |
| 83941   | 06/19/15 | 5647     | GRISWOLD,LASSALLE,COBB    | 1014060 | 031000  | \$67.50    |
| 83941   | 06/19/15 | 5647     | GRISWOLD,LASSALLE,COBB    | 1014060 | 031000  | \$123.60   |
| 83941   | 06/19/15 | 5647     | GRISWOLD,LASSALLE,COBB    | 1014060 | 031000  | \$276.90   |
| 83941   | 06/19/15 | 5647     | GRISWOLD,LASSALLE,COBB    | 1014060 | 031000  | \$334.00   |
| 83941   | 06/19/15 | 5647     | GRISWOLD,LASSALLE,COBB    | 1014060 | 031000  | \$534.30   |
| 83941   | 06/19/15 | 5647     | GRISWOLD,LASSALLE,COBB    | 1014060 | 031000  | \$2,713.60 |
| 83941   | 06/19/15 | 5647     | GRISWOLD,LASSALLE,COBB    | 1014060 | 031000  | \$3,985.70 |
| 83941   | 06/19/15 | 5647     | GRISWOLD,LASSALLE,COBB    | 1014060 | 031000  | \$7,602.93 |
| 84021   | 06/30/15 | 5918     | HAWK ANALYTICS            | 1014110 | 037004  | \$3,995.00 |
| 83942   | 06/19/15 | 3824     | HIGH SIERRA LUMBER        | 1014210 | 064002  | \$88.38    |
| 83942   | 06/19/15 | 3824     | HIGH SIERRA LUMBER        | 1014210 | 064002  | \$1,124.01 |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 1014210 | 022000  | \$265.03   |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 1014110 | 023000  | \$450.81   |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 8834883 | 022000  | \$60.95    |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 8844884 | 022000  | \$26.50    |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 8864886 | 022000  | \$10.60    |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 8874887 | 022000  | \$15.90    |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 8884888 | 022000  | \$47.70    |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 8894889 | 022000  | \$47.70    |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 8904890 | 022000  | \$10.60    |
| 84022   | 06/30/15 | 1391     | HOME DEPOT                | 8914891 | 022000  | \$45.05    |
| 84023   | 06/30/15 | 5881     | IHEART MEDIA              | 3004300 | 069084  | \$1,024.75 |
| 83943   | 06/19/15 | 5541     | JACK DAVENPORT SWEEPING   | 2614160 | 023001  | \$3,000.00 |
| 84024   | 06/30/15 | 192      | JAMES WINTON & ASSOCIATES | 6004180 | 065000  | \$1,282.50 |
| 84024   | 06/30/15 | 192      | JAMES WINTON & ASSOCIATES | 6004180 | 065000  | \$251.19   |
| 84025   | 06/30/15 | 3702     | JOB LARA                  | 3004300 | 055025  | \$582.00   |
| 84025   | 06/30/15 | 3702     | JOB LARA                  | 3004300 | 055025  | \$398.00   |
| 84025   | 06/30/15 | 3702     | JOB LARA                  | 3004300 | 055025  | \$50.00    |
| 84025   | 06/30/15 | 3702     | JOB LARA                  | 3004300 | 055025  | \$86.00    |
| 84026   | 06/30/15 | 5675     | JOHNNY GONZALEZ           | 3004300 | 055025  | \$582.00   |
| 84026   | 06/30/15 | 5675     | JOHNNY GONZALEZ           | 3004300 | 055025  | \$398.00   |
| 84026   | 06/30/15 | 5675     | JOHNNY GONZALEZ           | 3004300 | 055025  | \$50.00    |
| 84026   | 06/30/15 | 5675     | JOHNNY GONZALEZ           | 3004300 | 055025  | \$86.00    |
| 83944   | 06/19/15 | 2471     | L.N. CURTIS & SONS        | 1014110 | 024004  | \$49.68    |
| 83945   | 06/19/15 | 5701     | LAMAR COMPANIES           | 3004300 | 069084  | \$1,081.00 |

| CHECK # | DATE     | VENDOR # | VENDOR                         | FUND    | ACCOUNT | AMOUNT      |
|---------|----------|----------|--------------------------------|---------|---------|-------------|
| 83945   | 06/19/15 | 5701     | LAMAR COMPANIES                | 4004400 | 035000  | \$206.00    |
| 84027   | 06/30/15 | 5788     | LINCOLN AQUATICS               | 3004300 | 095002  | \$638.17    |
| 84028   | 06/30/15 | 4067     | LINCOLN NAT'L INSURANCE        | 101     | 200260  | \$509.41    |
| 83946   | 06/19/15 | 4427     | LINDSAY AUTO PARTS             | 1014120 | 022000  | \$7.06      |
| 84029   | 06/30/15 | 078      | LINDSAY CHAMBER OF COMMERCE    | 3004300 | 069115  | \$203.47    |
| 83947   | 06/19/15 | 218      | LINDSAY EQUIPMENT RENTAL       | 3004300 | 069092  | \$31.59     |
| 83948   | 06/19/15 | 1422     | LINDSAY TRUE VALUE             | 3004300 | 069092  | \$461.92    |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 1014110 | 023000  | \$4.32      |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 1014110 | 023000  | \$4.32      |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 1014110 | 023000  | \$2.25      |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 1014120 | 022000  | \$14.03     |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 1014120 | 023000  | \$31.29     |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 1014130 | 023000  | \$314.16    |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 1014120 | 022000  | \$430.31    |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 5524552 | 023000  | \$104.56    |
| 84030   | 06/30/15 | 1422     | LINDSAY TRUE VALUE             | 5534553 | 019000  | \$111.36    |
| 84031   | 06/30/15 | 2656     | LOURDES RENDON                 | 1014110 | 037000  | \$480.00    |
| 84032   | 06/30/15 | 5399     | MARCOS LOYA                    | 1014110 | 022015  | \$100.00    |
| 84033   | 06/30/15 | 5921     | MAYDWELL MASCOTS               | 3004775 | 064000  | \$4,500.00  |
| 84034   | 06/30/15 | 5852     | MID VALLEY DISPOSAL            | 5544554 | 065004  | \$68,256.16 |
| 84034   | 06/30/15 | 5852     | MID VALLEY DISPOSAL            | 5544554 | 065004  | \$68,272.10 |
| 84035   | 06/30/15 | 5625     | NGLIC--Superior Vision         | 101     | 200260  | \$634.90    |
| 84036   | 06/30/15 | 4204     | ORKIN PEST CONTROL             | 4004400 | 069091  | \$208.88    |
| 84037   | 06/30/15 | 5886     | OUTFRONT MEDIA                 | 3004300 | 069084  | \$500.00    |
| 84037   | 06/30/15 | 5886     | OUTFRONT MEDIA                 | 3004300 | 069084  | \$1,000.00  |
| 84038   | 06/30/15 | 5831     | PANADERIA LA FORTUNA           | 4004400 | 069116  | \$35.00     |
| 84039   | 06/30/15 | 5637     | PAPA MURPHY'S                  | 3004300 | 069116  | \$137.50    |
| 84039   | 06/30/15 | 5637     | PAPA MURPHY'S                  | 3004300 | 069116  | \$151.25    |
| 84039   | 06/30/15 | 5637     | PAPA MURPHY'S                  | 3004300 | 069116  | \$123.75    |
| 83949   | 06/19/15 | 3750     | PEPSI-COLA                     | 3004300 | 069116  | \$5,913.50  |
| 84040   | 06/30/15 | 3750     | PEPSI-COLA                     | 3004300 | 069116  | \$1,994.70  |
| 84040   | 06/30/15 | 3750     | PEPSI-COLA                     | 3004300 | 069116  | \$1,123.80  |
| 84040   | 06/30/15 | 3750     | PEPSI-COLA                     | 3004300 | 069116  | \$644.45    |
| 84040   | 06/30/15 | 3750     | PEPSI-COLA                     | 4004400 | 069116  | \$255.50    |
| 84041   | 06/30/15 | 269      | PHILLIPS HOME APPLIANCE        | 4004400 | 023000  | \$160.00    |
| 84042   | 06/30/15 | 5919     | PIERCE CONSTRUCTION            | 6004180 | 065000  | \$21,161.25 |
| 84044   | 06/30/15 | 2869     | PORTERVILLE SHELTERED WORKSHOP | 1014130 | 023000  | \$81.75     |
| 84044   | 06/30/15 | 2869     | PORTERVILLE SHELTERED WORKSHOP | 1014120 | 022000  | \$385.00    |
| 84044   | 06/30/15 | 2869     | PORTERVILLE SHELTERED WORKSHOP | 3004300 | 069084  | \$65.10     |
| 84044   | 06/30/15 | 2869     | PORTERVILLE SHELTERED WORKSHOP | 3004300 | 069101  | \$65.10     |
| 84044   | 06/30/15 | 2869     | PORTERVILLE SHELTERED WORKSHOP | 3004300 | 069101  | \$81.38     |
| 84044   | 06/30/15 | 2869     | PORTERVILLE SHELTERED WORKSHOP | 3004300 | 055026  | \$124.50    |
| 83951   | 06/19/15 | 1849     | PORTERVILLE VALLEY             | 1014110 | 031003  | \$85.00     |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY             | 1014120 | 031000  | \$80.00     |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY             | 3004300 | 069088  | \$210.00    |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY             | 3004300 | 069088  | \$70.00     |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY             | 4004400 | 037000  | \$70.00     |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY             | 4004400 | 037000  | \$70.00     |

| CHECK # | DATE     | VENDOR # | VENDOR                  | FUND    | ACCOUNT | AMOUNT     |
|---------|----------|----------|-------------------------|---------|---------|------------|
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY      | 4004400 | 037000  | \$70.00    |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY      | 4004400 | 037000  | \$70.00    |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY      | 4004400 | 037000  | \$70.00    |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY      | 4004400 | 037000  | \$70.00    |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY      | 4004400 | 037000  | \$70.00    |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY      | 4004400 | 037000  | \$25.00    |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY      | 4004400 | 037000  | \$45.00    |
| 84045   | 06/30/15 | 1849     | PORTERVILLE VALLEY      | 4004400 | 037000  | \$180.00   |
| 84046   | 06/30/15 | 4618     | PROVOST & PRITCHARD     | 5514552 | 038004  | \$288.00   |
| 83952   | 06/19/15 | 5522     | PURCHASE POWER          | 1014090 | 037000  | \$1,000.00 |
| 84047   | 06/30/15 | 284      | QUALITY MART            | 1014110 | 024000  | \$25.27    |
| 84048   | 06/30/15 | 5684     | QUIK-ROOTER             | 5534553 | 036001  | \$337.50   |
| 84048   | 06/30/15 | 5684     | QUIK-ROOTER             | 6004553 | 064002  | \$225.00   |
| 83953   | 06/19/15 | 285      | QUILL CORPORATION       | 1014110 | 021000  | \$30.68    |
| 83953   | 06/19/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$350.63   |
| 83953   | 06/19/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$19.49    |
| 83953   | 06/19/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$12.01    |
| 83953   | 06/19/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$13.82    |
| 83953   | 06/19/15 | 285      | QUILL CORPORATION       | 5524552 | 021000  | \$243.78   |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 1014120 | 021000  | \$64.87    |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 1014120 | 021000  | \$99.34    |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$411.42   |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$14.86    |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$35.09    |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$77.74    |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 3004300 | 069101  | \$110.15   |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 5524552 | 021000  | \$56.00    |
| 84049   | 06/30/15 | 285      | QUILL CORPORATION       | 5524552 | 021000  | \$101.47   |
| 83954   | 06/19/15 | 5717     | RANDSTAD/PLACEMENT PROS | 1014050 | 010008  | \$69.03    |
| 83954   | 06/19/15 | 5717     | RANDSTAD/PLACEMENT PROS | 5524552 | 010008  | \$345.19   |
| 83954   | 06/19/15 | 5717     | RANDSTAD/PLACEMENT PROS | 5534553 | 010008  | \$172.59   |
| 83954   | 06/19/15 | 5717     | RANDSTAD/PLACEMENT PROS | 5544554 | 010008  | \$103.58   |
| 84050   | 06/30/15 | 5717     | RANDSTAD/PLACEMENT PROS | 1014050 | 010008  | \$69.03    |
| 84050   | 06/30/15 | 5717     | RANDSTAD/PLACEMENT PROS | 5524552 | 010008  | \$345.19   |
| 84050   | 06/30/15 | 5717     | RANDSTAD/PLACEMENT PROS | 5534553 | 010008  | \$172.59   |
| 84050   | 06/30/15 | 5717     | RANDSTAD/PLACEMENT PROS | 5544554 | 010008  | \$103.58   |
| 83955   | 06/19/15 | 5356     | RAY MORGAN COMPANY      | 1014070 | 036008  | \$24.95    |
| 83955   | 06/19/15 | 5356     | RAY MORGAN COMPANY      | 1014050 | 036008  | \$32.15    |
| 83955   | 06/19/15 | 5356     | RAY MORGAN COMPANY      | 1014120 | 036008  | \$45.05    |
| 83955   | 06/19/15 | 5356     | RAY MORGAN COMPANY      | 3004300 | 069113  | \$328.41   |
| 84079   | 7/1/15   | 3840     | RICHARD RIOS            | 1014210 | 30001   | \$1,600.00 |
| 83956   | 06/19/15 | 3832     | RICK'S VENDING          | 3004300 | 069092  | \$417.28   |
| 83956   | 06/19/15 | 3832     | RICK'S VENDING          | 3004300 | 069090  | \$185.20   |
| 83956   | 06/19/15 | 3832     | RICK'S VENDING          | 3004300 | 069090  | \$199.00   |
| 83956   | 06/19/15 | 3832     | RICK'S VENDING          | 3004300 | 069090  | \$46.26    |
| 83956   | 06/19/15 | 3832     | RICK'S VENDING          | 3004300 | 069090  | -\$41.73   |
| 83957   | 06/19/15 | 298      | SAVE MART SUPERMARKET   | 1014010 | 037002  | \$11.48    |
| 83957   | 06/19/15 | 298      | SAVE MART SUPERMARKET   | 3004300 | 055026  | \$61.98    |

| CHECK # | DATE     | VENDOR # | VENDOR                 | FUND    | ACCOUNT | AMOUNT      |
|---------|----------|----------|------------------------|---------|---------|-------------|
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069103  | \$247.53    |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069103  | \$268.70    |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 055025  | \$263.97    |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069103  | \$201.90    |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069116  | \$7.38      |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069103  | \$8.55      |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069116  | \$7.00      |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069103  | \$35.31     |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069115  | \$38.87     |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069115  | \$71.08     |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069103  | \$76.91     |
| 84051   | 06/30/15 | 298      | SAVE MART SUPERMARKET  | 3004300 | 069103  | \$132.02    |
| 83958   | 06/19/15 | 4626     | SC COMMUNICATIONS      | 1014110 | 031001  | \$193.34    |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 1014050 | 031000  | \$1,111.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 1014050 | 031000  | \$1,111.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 1014050 | 031000  | \$1,111.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 1014050 | 031000  | \$2,152.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 1014050 | 031000  | \$2,160.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 1014050 | 031000  | \$2,168.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 7004700 | 031000  | \$7,500.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 7204720 | 031000  | \$2,152.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 7204720 | 096013  | \$44,000.00 |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 7204720 | 097012  | \$10,560.00 |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 7204720 | 031000  | \$2,168.00  |
| 84052   | 06/30/15 | 2168     | SELF-HELP ENTERPRISES  | 7204720 | 031000  | \$2,160.00  |
| 83959   | 06/19/15 | 5314     | SHRED-IT USA-FRESNO    | 1014090 | 037000  | \$60.08     |
| 83960   | 06/19/15 | 4555     | SIERRA CHEMICAL CO.    | 5524552 | 022007  | \$3,630.66  |
| 84053   | 06/30/15 | 5624     | SIERRA SANITATION, INC | 1014210 | 022000  | \$6.93      |
| 84053   | 06/30/15 | 5624     | SIERRA SANITATION, INC | 1014210 | 022000  | \$162.00    |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 1014120 | 022012  | \$168.27    |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 1014130 | 022012  | \$336.55    |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 1014120 | 022012  | \$336.55    |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 2614160 | 022012  | \$336.55    |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 5524552 | 022012  | \$336.55    |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 5534553 | 022012  | \$336.55    |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 8834883 | 022012  | \$58.79     |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 8844884 | 022012  | \$16.42     |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 8854885 | 022012  | \$4.51      |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 8864886 | 022012  | \$6.97      |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 8874887 | 022012  | \$10.67     |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 8884888 | 022012  | \$39.07     |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 8904890 | 022012  | \$2.42      |
| 83961   | 06/19/15 | 307      | SILVAS OIL COMPANY     | 8914891 | 022012  | \$29.41     |
| 84054   | 06/30/15 | 1776     | SMART & FINAL          | 3004300 | 069116  | \$310.53    |
| 84054   | 06/30/15 | 1776     | SMART & FINAL          | 3004300 | 055025  | \$39.98     |
| 84054   | 06/30/15 | 1776     | SMART & FINAL          | 4004400 | 069116  | \$119.39    |
| 84054   | 06/30/15 | 1776     | SMART & FINAL          | 4004400 | 069116  | \$150.30    |
| 84054   | 06/30/15 | 1776     | SMART & FINAL          | 4004400 | 069116  | \$375.01    |

| CHECK # | DATE     | VENDOR # | VENDOR                                  | FUND    | ACCOUNT | AMOUNT      |
|---------|----------|----------|---|---------|---------|-------------|
| 84054   | 06/30/15 | 1776     | SMART & FINAL                           | 4004400 | 069116  | \$184.38    |
| 84055   | 06/30/15 | 4573     | SNACK CLUB, INC.                        | 3004300 | 069116  | \$192.00    |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 1014130 | 032001  | \$123.86    |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 1014110 | 032001  | \$1,280.26  |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 1014120 | 032001  | \$1,914.36  |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 1014120 | 032001  | \$2,461.84  |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 2614160 | 032004  | \$6,397.14  |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 5524552 | 032006  | \$3,736.11  |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 5524552 | 032005  | \$13,649.22 |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 5534553 | 032001  | \$8,146.48  |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 5564556 | 022000  | \$394.99    |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8834883 | 032001  | \$166.66    |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8844884 | 032001  | \$26.75     |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8854885 | 032001  | \$26.75     |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8864886 | 032001  | \$44.76     |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8874887 | 032001  | \$73.25     |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8884888 | 032001  | \$26.75     |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8894889 | 032001  | \$55.48     |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8904890 | 032001  | \$102.31    |
| 83962   | 06/19/15 | 310      | SOUTHERN CA. EDISON                     | 8914891 | 032002  | \$404.35    |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 1014210 | 032001  | \$26.78     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 1014210 | 032001  | \$48.25     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 2614160 | 032004  | \$102.29    |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 2614160 | 032004  | \$99.44     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 2614160 | 032004  | \$86.72     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 2614160 | 032004  | \$77.19     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 2614160 | 032004  | \$64.42     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 2614160 | 032004  | \$33.42     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 2614160 | 032004  | \$231.38    |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 3004300 | 069108  | \$22,756.11 |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 3004300 | 069108  | \$34.35     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 8914891 | 032001  | \$26.78     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 8914891 | 032001  | \$26.90     |
| 84056   | 06/30/15 | 310      | SOUTHERN CA. EDISON                     | 8914891 | 032001  | \$27.14     |
| 84057   | 06/30/15 | 5855     | SPORTS TROPHIES & SPORTING GOODS        | 3004300 | 055019  | \$248.97    |
| 84058   | 06/30/15 | 5922     | SPOT COOLERS                            | 3004775 | 064000  | \$39,677.44 |
| 83963   | 06/19/15 | 5691     | STATE WATER RESOURCE                    | 5524552 | 038002  | \$65.00     |
| 84059   | 06/30/15 | 5267     | SUNGARD PUBLIC SECTOR                   | 1014120 | 037000  | \$605.00    |
| 84059   | 06/30/15 | 5267     | SUNGARD PUBLIC SECTOR                   | 1014050 | 037000  | \$605.00    |
| 84059   | 06/30/15 | 5267     | SUNGARD PUBLIC SECTOR                   | 3004300 | 037000  | \$605.00    |
| 84059   | 06/30/15 | 5267     | SUNGARD PUBLIC SECTOR                   | 5524552 | 037000  | \$605.00    |
| 84059   | 06/30/15 | 5267     | SUNGARD PUBLIC SECTOR                   | 5534553 | 019000  | \$605.00    |
| 83964   | 06/19/15 | 5899     | SUPPLYWORKS                             | 3004300 | 069092  | \$863.84    |
| 83964   | 06/19/15 | 5899     | SUPPLYWORKS                             | 3004300 | 069091  | \$59.97     |
| 83964   | 06/19/15 | 5899     | SUPPLYWORKS                             | 3004300 | 069091  | \$6.67      |
| 84060   | 06/30/15 | 5899     | SUPPLYWORKS                             | 3004300 | 069091  | \$324.75    |
| 83965   | 06/19/15 | 5646     | Specialized Utility Services Prgrm Inc. | 5524552 | 031000  | \$2,050.00  |
| 83965   | 06/19/15 | 5646     | Specialized Utility Services Prgrm Inc. | 5534553 | 031000  | \$2,050.00  |

| CHECK # | DATE     | VENDOR # | VENDOR                     | FUND    | ACCOUNT | AMOUNT     |
|---------|----------|----------|----------------------------|---------|---------|------------|
| 83966   | 06/19/15 | 3682     | SYSCO OF CENTRAL CA        | 3004300 | 069116  | \$737.86   |
| 84061   | 06/30/15 | 3682     | SYSCO OF CENTRAL CA        | 3004300 | 069116  | \$547.28   |
| 84061   | 06/30/15 | 3682     | SYSCO OF CENTRAL CA        | 3004300 | 069116  | \$579.93   |
| 84061   | 06/30/15 | 3682     | SYSCO OF CENTRAL CA        | 3004300 | 069116  | \$590.21   |
| 84061   | 06/30/15 | 3682     | SYSCO OF CENTRAL CA        | 3004300 | 069116  | \$1,280.81 |
| 84062   | 06/30/15 | 5755     | TELEPACIFIC COMMUNICATIONS | 1014120 | 033001  | \$443.48   |
| 84062   | 06/30/15 | 5755     | TELEPACIFIC COMMUNICATIONS | 1014130 | 033001  | \$443.48   |
| 84062   | 06/30/15 | 5755     | TELEPACIFIC COMMUNICATIONS | 1014210 | 033001  | \$443.49   |
| 84062   | 06/30/15 | 5755     | TELEPACIFIC COMMUNICATIONS | 5524552 | 033001  | \$443.48   |
| 84062   | 06/30/15 | 5755     | TELEPACIFIC COMMUNICATIONS | 5534553 | 033001  | \$443.48   |
| 84062   | 06/30/15 | 5755     | TELEPACIFIC COMMUNICATIONS | 5544554 | 033001  | \$443.48   |
| 83940   | 06/19/15 | 144      | THE GAS COMPANY            | 1014120 | 032002  | \$17.70    |
| 83940   | 06/19/15 | 144      | THE GAS COMPANY            | 1014120 | 032002  | \$18.66    |
| 83940   | 06/19/15 | 144      | THE GAS COMPANY            | 1014110 | 032002  | \$42.82    |
| 83940   | 06/19/15 | 144      | THE GAS COMPANY            | 3004300 | 069109  | \$180.75   |
| 83940   | 06/19/15 | 144      | THE GAS COMPANY            | 3004300 | 069109  | \$132.56   |
| 83940   | 06/19/15 | 144      | THE GAS COMPANY            | 4004400 | 032006  | \$989.32   |
| 83967   | 06/19/15 | 5792     | THOMPSON REUTERS -WEST     | 1014110 | 037004  | -\$147.21  |
| 83967   | 06/19/15 | 5792     | THOMPSON REUTERS -WEST     | 1014110 | 037004  | \$150.15   |
| 83967   | 06/19/15 | 5792     | THOMPSON REUTERS -WEST     | 1014110 | 037004  | \$150.15   |
| 84063   | 06/30/15 | 4265     | TROPICAL FOODS             | 3004300 | 069116  | \$303.12   |
| 84064   | 06/30/15 | 1664     | TU CO ENVIRONMENTAL HEALTH | 4004400 | 032007  | \$54.10    |
| 84064   | 06/30/15 | 1664     | TU CO ENVIRONMENTAL HEALTH | 4004400 | 032007  | \$38.30    |
| 84065   | 06/30/15 | 473      | TU CO RESOURCE MANAGEMENT  | 5524552 | 037004  | \$208.00   |
| 84066   | 06/30/15 | 4767     | TULARE REGIONAL MEDICAL    | 3004300 | 055025  | \$232.00   |
| 84066   | 06/30/15 | 4767     | TULARE REGIONAL MEDICAL    | 3004300 | 055025  | \$50.00    |
| 84067   | 06/30/15 | 4849     | U.S. BANK EQUIPMENT        | 1014110 | 031000  | \$1,574.02 |
| 84067   | 06/30/15 | 4849     | U.S. BANK EQUIPMENT        | 1014110 | 031000  | \$2,462.85 |
| 83968   | 06/19/15 | 1513     | UNITED RENTALS, INC        | 6004553 | 064002  | \$723.60   |
| 83969   | 06/19/15 | 5747     | UNITED STAFFING            | 1014130 | 022000  | \$1,119.98 |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 1014210 | 022000  | \$441.00   |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 1014110 | 022000  | \$441.00   |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 1014210 | 022000  | \$753.37   |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 1014110 | 022000  | \$753.37   |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8834883 | 022000  | \$172.20   |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8834883 | 022000  | \$100.80   |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8844884 | 022000  | \$63.00    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8844884 | 022000  | \$107.62   |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8864886 | 022000  | \$12.60    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8864886 | 022000  | \$21.52    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8874887 | 022000  | \$37.80    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8874887 | 022000  | \$64.57    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8884888 | 022000  | \$75.60    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8884888 | 022000  | \$129.15   |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8894889 | 022000  | \$21.53    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8894889 | 022000  | \$12.60    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8914891 | 022000  | \$75.60    |
| 84068   | 06/30/15 | 5747     | UNITED STAFFING            | 8914891 | 022000  | \$129.15   |

| CHECK # | DATE     | VENDOR # | VENDOR                          | FUND    | ACCOUNT | AMOUNT       |
|---------|----------|----------|---------------------------------|---------|---------|--------------|
| 84069   | 06/30/15 | 5413     | UNIVAR USA INC                  | 5524552 | 022007  | \$390.92     |
| 84069   | 06/30/15 | 5413     | UNIVAR USA INC                  | 5524552 | 022004  | \$594.12     |
| 84070   | 06/30/15 | 356      | USA BLUEBOOK                    | 5534553 | 019000  | \$169.06     |
| 84071   | 06/30/15 | 4865     | VALLEY ELECTRICAL SUPPLIERS     | 3004300 | 069092  | \$81.00      |
| 83970   | 06/19/15 | 1010     | VERIZON CALIFORNIA              | 1014110 | 033001  | \$72.20      |
| 83970   | 06/19/15 | 1010     | VERIZON CALIFORNIA              | 1014110 | 033001  | \$2,562.71   |
| 83970   | 06/19/15 | 1010     | VERIZON CALIFORNIA              | 3004300 | 069111  | \$119.32     |
| 83970   | 06/19/15 | 1010     | VERIZON CALIFORNIA              | 5524552 | 033001  | \$333.23     |
| 83970   | 06/19/15 | 1010     | VERIZON CALIFORNIA              | 5534553 | 033001  | \$286.62     |
| 84072   | 06/30/15 | 1010     | VERIZON CALIFORNIA              | 3004300 | 069111  | \$3.88       |
| 83971   | 06/19/15 | 1041     | VERIZON WIRELESS                | 5524552 | 033001  | \$71.17      |
| 84073   | 06/30/15 | 3645     | VISALIA UNIFIED SCHOOL DISTRICT | 3004300 | 055026  | \$980.62     |
| 84074   | 06/30/15 | 370      | WALMART COMMUNITY               | 3004300 | 069090  | \$395.10     |
| 84074   | 06/30/15 | 370      | WALMART COMMUNITY               | 3004300 | 055026  | \$73.60      |
| 84074   | 06/30/15 | 370      | WALMART COMMUNITY               | 3004300 | 069091  | \$123.20     |
| 84075   | 06/30/15 | 612      | WEISENBERGERS ACE HARDWARE      | 3004300 | 069092  | \$477.40     |
| 84075   | 06/30/15 | 612      | WEISENBERGERS ACE HARDWARE      | 3004300 | 069092  | \$433.95     |
| 83972   | 06/19/15 | 5732     | WILBUR-ELLIS COMPANY            | 1014120 | 022000  | \$269.46     |
| 84076   | 06/30/15 | 2790     | WILLDAN INC.                    | 1014120 | 031000  | \$2,550.00   |
| 84077   | 06/30/15 | 5780     | WILLIAM FOXLEY, MD              | 3004300 | 055025  | \$216.00     |
| 84077   | 06/30/15 | 5780     | WILLIAM FOXLEY, MD              | 3004300 | 055025  | \$398.00     |
|         |          |          |                                 |         |         |              |
|         |          |          |                                 |         |         |              |
|         |          |          |                                 |         | TOTAL   | \$642,638.48 |



## Treasurer's Report

JUNE 2015

### Cash Balances Classified by Depository

|  | GL ACT# | TYPE    | Balance               | Current Annual Yield | Interest Earned This Period |
|--|---------|---------|-----------------------|----------------------|-----------------------------|
| Petty Cash/Cash Register Funds             | 100-102 | RES     | \$2,628.00            | N/A                  |                             |
| Asset Forfeiture CD - BOTS                 | 100-110 | INV-RES | \$54,151.24           | 0.43                 |                             |
| Bank Of Sierra - Payroll                   | 100-106 | GEN     | \$143,767.55          | N/A                  |                             |
| Bank Of Sierra - AP/Operating              | 100-100 | GEN     | \$185,793.64          | N/A                  |                             |
| Bank Of Sierra - McDermont                 | 100-500 | GEN     | \$86,220.42           | N/A                  |                             |
| Bank of Sierra - 90 Day Investment CD      | 100-117 | INV-RES | \$238,366.16          | 0.45                 | 100.56                      |
| Bank Of Sierra - Impound Account           | 100-120 | RES     | \$8,454.51            | 0.00                 |                             |
| Bank Of Sierra - WWTP Project              | 100-553 | RES     | \$150,241.82          | 0.00                 |                             |
| Bank Of Sierra - Water Project             | 100-552 | RES     | \$110,355.33          | 0.00                 |                             |
| LAIF Savings: City                         | 100-103 | INV-RES | \$100,806.05          | 0.29                 | 161.47                      |
| <b>Total Cash Balances @ June 30, 2015</b> |         |         | <b>\$1,080,784.72</b> |                      | <b>262.03</b>               |

|                                    |           |                           |            |                   |
|------------------------------------|-----------|---------------------------|------------|-------------------|
| JUNE 2015 Accounts Payable         | \$974,488 | <b>JUNE DEBT SERVICE:</b> |            | <b>-25,623.00</b> |
| JUNE Payroll & Benefits 06/12/2015 | \$245,369 |                           |            |                   |
| JUNE Payroll & Benefits 06/26/2015 | \$275,540 | Sewer Infrastructure Loan | USDA 92-09 | -25,623.00        |

**JUNE 2015 Total Expenditures** **\$1,495,397**

#### Compliance with Investment Policy

As of June 30, 2015, the investments were in compliance with the requirements of the City's investment policy. This report reflects all cash and investments of the City of Lindsay (*excluding bond reserves*). There are sufficient funds to meet the City's expenditure requirements for the following month.

Respectfully submitted,

Tamara Laken  
Finance Director/City Treasurer

GEN=GENERAL UNRESTRICTED  
RES=RESTRICTED ACTIVITY  
INV=INVESTMENT

Invested Funds  
\$393,323.45



DATE : July 14, 2015  
TO : Honorable Mayor Padilla and City Council Members  
FROM : Michael Camarena, City Services Director  
RE : Project Acceptance as Completed; 2015-2 Waste Water Treatment Plant  
Headwork's Renovation Project

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**PURPOSE**

99 Pipeline, Inc has completed the Waste Water Treatment Plant Headwork's Renovation Project and has requested acceptance at this time.

Staff has reviewed the projects and is satisfied that the work has been completed in accordance with the contract documents.

**FINANCIAL REPORT**

|                         |              |
|-------------------------|--------------|
| Project Budget:         | \$165,000.00 |
| Contract Amount:        | \$133,640.00 |
| Contract Amendments:    | \$ 0.00      |
| Contract Change Orders: | \$ 578.00    |
| Total Contract:         | \$134,218.00 |

**RECOMMENDED ACTION**

Staff recommends acceptance of the 2015-2 Waste Water Treatment Plant Headwork's Renovation Project as completed and direct the City Clerk to file a "Notice of Completion" with the County Recorder. 1 year warranty period will begin upon recordation.

**Attachments:**

1. None

**EMPLOYMENT AGREEMENT**

**EMPLOYMENT AGREEMENT BETWEEN  
THE CITY OF LINDSAY AND WILLIAM ZIGLER**

WHEREAS, the City of Lindsay, hereafter referred to as “City” desires to hire William Zigler (“Zigler”), as its Interim City Manager;

WHEREAS, Zigler, desires to be employed as the Interim City Manager of the City of Lindsay;

WHEREAS, it is the desire and intent of the parties to create an Employment Agreement;

WHEREAS, the City desires to employ the services of Zigler, as Interim City Manager of the City as provided by Section 4.01 of the Charter of the City of Lindsay;

WHEREAS, it is the desire of the City Council to provide certain benefits, establish certain conditions of employment and to set working conditions of the Interim City Manager; and

WHEREAS, it is the desire of the City Council to:

- (1) Secure and retain the services of the Interim City Manager and provide inducement for him to remain in such employment;
- (2) To make possible full work productivity by assuring the Interim City Manager’s morale and peace of mind with respect to future security; and
- (3) To provide a just means for terminating the Interim City Manager’s services at such time as he may be unable to discharge his duties or when the City may desire to otherwise terminate his employment.

NOW, THEREFORE, in consideration of the mutual covenants herein contained, the parties agree to enter into this Employment Agreement, as follows:

**Section 1. Compensation.**

Commencing July 14, 2015, and thereafter, City agrees to employ and pay the Interim City Manager at the rate of five percent (5%) per month above his base salary as Planning and Economic Development Director, as compensation for the retention of the above mentioned services, and such compensation thereafter as the City Council may, from time to time, determine. Said compensation shall be paid at the same times and in the same manner as other employees of the City are paid. The City Council shall additionally allow the Interim City Manager to maintain his position as Planning and Economic Development Director, as authorized in Section 7.01(E) of the Lindsay Municipal Charter.

**Section 2. Term of Service.**

The Interim City Manager’s term of employment shall be indefinite. Nothing in this Employment Agreement shall prevent, limit or otherwise interfere with the right of the City

Council to terminate the services of the Interim City Manager at any time. The Interim City Manager may be removed by a majority vote of the total City Council Members. Before voluntarily resigning his position, the Interim City Manager agrees to give the City Council at least one (1) months' notice in writing of his intention to resign stating the reasons therefore. It is specifically noted and agreed that Interim City Manager's employment with the City is in an "at will" capacity and as such, he may be terminated at any time, with or without cause. Any termination of Interim City Manager shall be made by giving him written notice ten (10) days in advance.

As this contract is for an interim position, should Zigler resign or be replaced by the Council, Zigler shall have the option to resume his duties solely as Planning and Economic Development Director, and serve out the original terms of the contract entered therefore.

### **Section 3. Dues, Travel and Conferences.**

The City hereby agrees to annually budget and allocate sufficient funds to pay for the dues and expenses of the Interim City Manager's necessary travel and living while representing the City at the Annual League of California Cities Conference, conferences of the City Manager's Department of the League of California Cities and conferences or meetings of state committees or commissions upon which the Manager serves as a member, said membership on said committees or commissions being subject to the approval of the Council, and for such other official conferences, meetings and/or travel as are reasonably necessary for the Interim City Manager to carry out his professional responsibilities as the appointed executive of the City.

### **Section 4. Regular Benefits.**

All provisions of the City Charter and Code and Rules and Regulations of the City relating to vacation, sick leave, retirement and pension system contributions, holidays and other fringe benefits and working conditions as they now exist or hereafter may be amended shall apply to the Interim City Manager as they would to other management employees of the City, in addition to any other benefits enumerated herein specifically for the benefit of the Interim City Manager, except as otherwise provided in this Employment Agreement.

### **Section 5. Supplemental Benefits.**

In addition to the regular benefit provided for in Section 4 herein above, the Interim City Manager shall be entitled to the following supplemental benefits:

- a. He shall be enrolled in the PERS retirement system and the City shall pay the employer's and the employee's required contributions to said Program; however in the event employer's and/or employee's contributions increase, Interim City Manager shall pay the increase to the extent it can be attributed to the employee's portion of required contributions.
- b. He shall be entitled to participate in the City employee's deferred compensation program, receiving a maximum contribution of 7.5% of his annual salary by the City;
- c. Interim City Manager shall accrue, and have credited to his personal account, vacation and sick leave at the same rate as other upper management level

employees. Interim City Manager may not accrue more than 300 hours combined vacation and administrative leave annually. Interim City Manager must take or forfeit any hour accrued above the 300 hour limit. Annual leave pay-outs shall be allowed only for hours in excess of 200;

- d. The City shall pay for Interim City Manager's monthly cellular telephone service; provided that he shall be available and accessible to City Council Members by means of these communication devices seven (7) days a week, twenty four (24) hours a day, unless he has first otherwise notified the Council of his unavailability for an approved reason or basis.

#### **Section 6. General Expenses.**

The City recognizes that certain expenses of a non-personal job related nature will be incurred by the Interim City Manager while on City business. Such expenses include, but are not limited to, meals with potential tenants, developers, representatives of businesses interested in locating in the City, or already located in the City, and other federal, state and local agency officials. The City agrees to reimburse or to pay said general expenses to the extent legally permitted and the Finance Director is hereby authorized to disperse such money upon receipt of duly executed expense or petty cash vouchers, receipts, statements or personal affidavits. Such receipts and supporting documentation shall be made available to the Mayor and City Council, and the City Council may designate one of its members or another individual to audit the Interim City Manager's expenses. The Interim City Manager shall provide the City Council with all of his expense and reimbursement records for each year prior to his annual performance evaluation.

#### **Section 7. Automobile Allowance.**

The City agrees to provide the Interim City Manager with a vehicle allowance in the amount of \$400.00 per month, plus reimbursement for 2 tanks of gas per month. This allowance shall not be considered part of Interim City Manager's salary for the purposes of PERS final compensation.

#### **Section 8. Civic Club Membership.**

The City recognizes the desirability of representation in and before local civic and other organizations and the Interim City Manager is authorized to become a member of such club(s) and organization(s). City shall pay related and reasonable expenses thereto and allow the Interim City Manager to participate on City time.

#### **Section 9. Other Terms and Conditions of Employment.**

The parties shall, by amendment to this Employment Agreement, fix such other terms and conditions of employment from time to time as may be determined relating to the performance of the Interim City Manager of the City, provided such terms and conditions are not inconsistent or in conflict with the provisions of this Employment Agreement or City Charter.

#### **Section 10. General Provisions.**

If a provision or any portion thereof contained in this Employment Agreement is held to

be unconstitutional, invalid or unenforceable, the remainder of this Employment Agreement or portion thereof shall be deemed to be severable, shall not be affected and shall remain in full force and effect. This Employment Agreement supersedes any and all prior Employment Agreements between the parties. This Employment Agreement is entered in to and is to be performed in Tulare County, California.

Dated this 14<sup>th</sup> day of July, 2015.

**CITY OF LINDSAY**

**INTERIM CITY MANAGER**

---

Ramona Villarreal-Padilla  
Mayor of the City of Lindsay

---

William Zigler

**ATTEST:**

---

Carmela Wilson  
City Clerk and Clerk of the Council of the City  
of Lindsay

**APPROVED AS TO FORM:**

GRISWOLD, LaSALLE, COBB,  
DOWD & GIN, LLP

---

MARIO U. ZAMORA  
City Attorney

## PUBLIC HEARING PROCEDURES

The following rules shall apply:

1. OPEN the public hearing.
2. PROPOSERS (those in favor). The Council may ask questions of the proponents and they may respond.
3. OPPOSERS (those against). The Council may ask questions of the opponents and they may respond.
4. REBUTTAL each side.
5. FURTHER QUESTIONS from Council, but the parties may not engage in further debate.
6. CLOSE the public hearing.
7. COUNCIL DISCUSSION.
8. MOTION ( if necessary).
9. COUNCIL VOTE.

**CITY OF LINDSAY  
STAFF REPORT  
MODIFICATION OF PARCEL MAP 3487 (PPN 15-30)  
July 14, 2015**

**GENERAL INFORMATION**

- |                      |   |
|----------------------|---|
| 1. Applicant:        | Maninder S. Sandhu<br>1163 E. Pinehurst Ave.<br>Fresno, CA 93730                |
| 2. Requested Action: | Modification of Parcel Map No. 3487   |
| 3. Location:         | Northeast corner of Hwy 65 and Hermosa Street (APNs: 199-280-001 & 199-280-002) |

**PROJECT DESCRIPTION**

Modification of Parcel Map No. 3487 (map attached) is a request by Maninder S. Sandhu to remove the requirement of a thirty-foot wide building setback line located along the easterly line of parcels 1 and 2 due to the setback being no longer appropriate or necessary. A certificate of map correction (certificate attached) has been submitted by a licensed surveyor for map modification. If the requested modification is approved by the City Council at a noticed public hearing, current building setback requirements for the highway commercial (CH) zoning district would apply.

**SITE BACKGROUND**

**Background:** Parcel Map 3487 was created in 1987 to split a 3.14 acre lot into two parcels to accommodate a motel (currently, the Super 8 Motel) to the north and a restaurant (currently, the Country Waffle Restaurant) to the south. Parcel 1 (Super 8) is 2.19 acres in area and Parcel 2 (Country Waffle) is .95 acres. Parcel Map 3487 included a non-exclusive easement for driving, parking, and walking in areas designated as the parking lot and adjoining driveways and sidewalks as well as a reciprocal drainage easement. Condition six of resolution CC 86-62 (attached), which approved the map, required that Parcel Map 3487 additionally create a thirty-foot wide building setback line located along the easterly line of parcels 1 and 2 to “reserve an area, free from structural improvement, all or part of which could be acquired with other frontage road street acquisition if and when necessary.” This appears to have been intended to reserve an area for a one-half street right-of-way (ROW) along the eastern property lines of the parcels created by Parcel Map 3487.

Parcel Map 4310 (attached) was created in 1997 to subdivide the adjacent property to the east; however, no similar condition was placed on this map to facilitate the creation of a frontage road.

Surrounding land uses for the project site include:

- North: Vacant land
- South: Hermosa Street and to the south of Hermosa Street, the Olivewood Plaza
- East: Burger King (highway commercial use) and vacant land
- West: Highway 65 and to the west of Hwy 65, vacant land

### **COMPATIBILITY WITH EXISTING ZONING, PLANS AND POLICIES**

**Zoning and Land Use:** The proposed project is consistent with the policies, objectives, and standards of the Lindsay General Plan, the Subdivision Ordinance and the Zoning Ordinance. Specifically, the map modification would align site building setback requirements with other similarly zoned properties within the highway commercial (CH) district.

### **PROJECT EVALUATION**

The thirty-foot setback requirement along the easterly line of parcels 1 and 2 is unusual and is not in conformance with current zoning code section 18.10.070E, which allows commercial buildings to be built up to the property line when not abutting an RCO, UR, R, RM, or PO zoning district. The project site abuts a similarly zoned highway commercial (CH) parcel and would qualify for zero setback per current zoning code.

Because no similar requirement was placed on Parcel Map 4310 for the adjacent property a proper street cannot be created in this area without new and independent actions of condemnation on the part of City Council. It should be noted that no street was planned for this area on the 1989 General Plan map, which was developed following the CC 86-62 resolution requirement.

Section 17.24.160 of newly adopted Subdivision Ordinance 549 lists several findings that are needed for approving a parcel map modification. They are provided below and in the attached resolution:

- There are changes in circumstances that make any or all of the conditions of the map no longer appropriate or necessary.
- The modifications do not impose any additional burden on the fee owners of the real property.
- The modifications do not alter any right, title, or interest in the real property reflected on the recorded map.
- The map, as modified, conforms to Section 66474 of the Subdivision Map Act, which states:

A legislative body of a city or county shall deny approval of a tentative map, or a parcel map for which a tentative map was not required, if it makes any of the following findings:

- (a) That the proposed map is not consistent with applicable general and specific plans as specified in Section 65451.
- (b) That the design or improvement of the proposed subdivision is not consistent with applicable general and specific plans.

- (c) That the site is not physically suitable for the type of development.
- (d) That the site is not physically suitable for the proposed density of development.
- (e) That the design of the subdivision or the proposed improvements are likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.
- (f) That the design of the subdivision or type of improvements is likely to cause serious public health problems.
- (g) That the design of the subdivision or the type of improvements will conflict with easements, acquired by the public at large, for access through or use of, property within the proposed subdivision. In this connection, the governing body may approve a map if it finds that alternate easements, for access or for use, will be provided, and that these will be substantially equivalent to ones previously acquired by the public. This subsection shall apply only to easements of record or to easements established by judgment of a court of competent jurisdiction and no authority is hereby granted to a legislative body to determine that the public at large has acquired easements for access through or use of property within the proposed subdivision.

## **ENVIRONMENTAL REVIEW**

The project is Categorically Exempt from CEQA as a Minor Alteration in Land Use Limitations, Article 19, Section 15305. A CEQA Notice of Exemption has been prepared and has been available for public review at the City of Lindsay Planning and Economic Development Department.

## **RECOMMENDATION**

Staff recommends approval of the modification of Parcel Map No. 3487 (PPN 15-30) due to the following:

- Retention of the existing thirty foot building setback requirement would serve no good purpose for the public or the property owner because a proper street cannot be developed in this area without new and independent actions of condemnation on the part of City Council of an adjacent developed property.
- The current General Plan does not identify a street for this area.
- The map modification satisfies all required findings of the Subdivision Map Act and City of Lindsay Ordinance 549.

## **ATTACHMENTS**

- Parcel Map 3487
- Certificate of Map Correction
- Resolution CC 86-62
- Parcel Map 4310
- Draft Resolution 15-34

OWNER'S CERTIFICATE

WE HEREBY CERTIFY THAT WE ARE THE LEGAL OWNER'S OF OR HAVE SOME RIGHT TITLE OR INTEREST IN AND TO THE REAL PROPERTY SHOWN ON THIS SUBDIVISION MAP AND THAT WE ARE THE ONLY PERSONS WHOSE CONSENT IS NECESSARY TO PASS A CLEAR TITLE TO SAID REAL PROPERTY AND THAT WE CONSENT TO THE MAKING AND FILING OF SAID PARCEL MAP AS SHOWN WITHIN THE BLUE BORDER HEREON.

Roger W. Yates 12/30/86  
ROGER W. YATES DATE

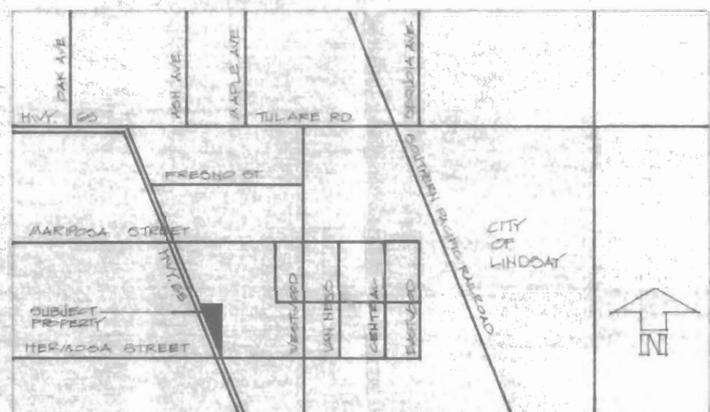
Carole S. Yates 12/30/86  
CAROLE S. YATES DATE

Ken N. Swanson 12/30/86  
KENNETH N. SWANSON DATE

Darla J. Swanson 12/30/86  
DARLA J. SWANSON DATE

PARCEL MAP NO. 3487  
LINDSAY, CALIFORNIA

BEING A DIVISION OF PARCEL NO. 2 OF PARCEL MAP NO. 3282 AS PER MAP RECORDED IN BOOK 33 AT PAGE 83 OF PARCEL MAPS IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, LOCATED IN THE NORTHEAST QUARTER OF SECTION 12, TOWNSHIP 29 SOUTH, RANGE 26 EAST, MOUNT DIABLO BASE AND MERIDIAN, IN THE CITY OF LINDSAY, COUNTY OF TULARE, STATE OF CALIFORNIA.



LOCATION MAP

NOT TO SCALE

ENGINEER'S CERTIFICATE

STATE OF CALIFORNIA  
COUNTY OF TULARE

THIS MAP WAS PREPARED BY ME OR UNDER MY DIRECTION AND IS BASED UPON A FIELD SURVEY IN CONFORMANCE WITH THE REQUIREMENTS OF THE SUBDIVISION MAP ACT AND LOCAL ORDINANCE AT THE REQUEST OF KEN SWANSON AND ROGER YATES. I HEREBY STATE THAT THE PARCEL MAP PROCEDURES OF THE LOCAL AGENCY HAVE BEEN COMPLIED WITH AND THAT THIS PARCEL MAP CONFORMS TO THE APPROVED TENTATIVE MAP AND CONDITIONS OF APPROVAL THEREOF, WHICH WERE REQUIRED TO BE FULFILLED PRIOR TO THE FILING OF THE PARCEL MAP.



Wayne F. Harris  
WAYNE F. HARRIS, R.C.E. 15342, EXP. 3/31/89

CITY ENGINEER'S CERTIFICATE

STATE OF CALIFORNIA  
COUNTY OF TULARE

THIS MAP CONFORMS WITH THE REQUIREMENTS OF THE SUBDIVISION MAP ACT AND LOCAL ORDINANCE

DATED 2/13/87



John S. Dutton  
JOHN S. DUTTON, R.C.E. 24673 EXP. DATE 12/31/89

PREPARED BY  
WAYNE F. HARRIS & ASSOCIATES, INC.

101 WEST MORTON AVENUE  
PORTERVILLE, CALIFORNIA 93257  
(209) 781-2268

FOR  
KEN SWANSON & ROGER YATES

3434 TRUXTON AVENUE, SUITE 200  
BAKERSFIELD, CA. 93301  
(805) 322-7447

PLANNING DIRECTOR'S CERTIFICATE

I HEREBY CERTIFY, ON BEHALF OF THE PLANNING COMMISSION, THAT THE FINAL MAP CONFORMS TO THE APPROVED TENTATIVE PARCEL MAP NO. 3487 THIS 17th DAY OF FEBRUARY, 1987.

Gary E. Irish  
DIRECTOR OF PLANNING

NOTARY CERTIFICATE

STATE OF CALIFORNIA  
COUNTY OF FERN

ON THIS 30 DAY OF DECEMBER, 1986  
BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, PERSONALLY APPEARED ROGER W. YATES, CAROLE S. YATES, KENNETH N. SWANSON AND DARLA J. SWANSON

KNOWN TO ME TO BE THE PERSON(S) WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE WITHIN INSTRUMENT AND ACKNOWLEDGED THAT THEY EXECUTED THE SAME THE SAME FOR THE PURPOSES THEREIN CONTAINED.

IN WITNESS OF, I HEREBY SET MY HAND AND OFFICIAL SEAL

Dawn Haven 12/30/86  
SIGNATURE DATE



BOARD OF SUPERVISOR'S

I, JOHN C. McCLURE, II, COUNTY EXECUTIVE/CLERK OF THE BOARD OF SUPERVISORS OF THE COUNTY OF TULARE, DO HEREBY CERTIFY THAT SAID BOARD OF SUPERVISORS HAS APPROVED THE PROVISIONS MADE FOR THE PAYMENT OF TAXES AS PROVIDED IN DIVISION 2 OF TITLE 7 OF THE GOVERNMENT CODE OF THE STATE OF CALIFORNIA.

JOHN C. McCLURE, II, COUNTY EXECUTIVE/CLERK



BY Justin J. Schaid  
DEPUTY

COUNTY RECORDER'S CERTIFICATE

FILED THIS 20th DAY OF February, 1987  
IN VOLUME 35 OF MAPS, PAGE 89 AT THE  
REQUEST OF Safeco Title Insurance Co.

FEE NUMBER 9456  
FEE PAID \$8.00

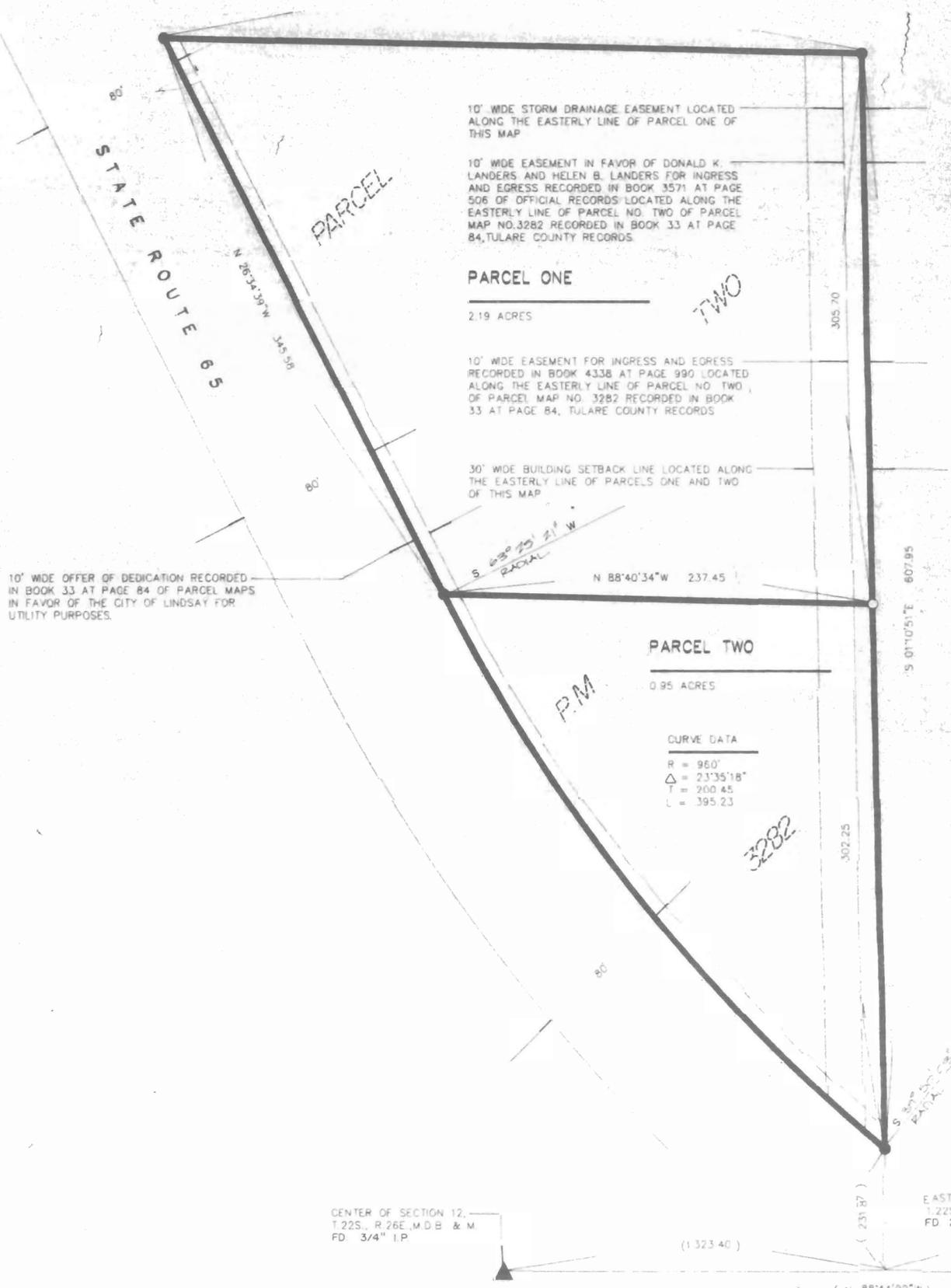
JAY C. BAYLESS, COUNTY RECORDER

BY Nadine Burgoon  
DEPUTY

2-18-87

PARCEL ONE OF PARCEL MAP NO. 3282  
RECORDED IN BOOK 33, PAGE 84, T.C.R.

N 88°40'34"W 385.80



**LEGEND**

- FOUND 3/4" I.P. TAGGED R.C.E. 15342
- SET 3/4" I.P. TAGGED R.C.E. 15342
- ( 00.00 ) PER P.M. NO. 3282 REC. IN BK. 33, PG. 84, T.C.R.

**BASIS OF BEARING**

BEING THE NORTH LINE OF PARCEL 2 OF PARCEL MAP NO. 3282, RECORDED IN BOOK 33, PAGE 84 OF PARCEL MAPS, LOCATED IN THE CITY OF LINDSAY, COUNTY OF TULARE, STATE OF CALIFORNIA, BEARING N 88° 40' 34" W.



**RECIPROCAL PARKING EASEMENT**

PARCEL ONE AND PARCEL TWO HEREBY GRANTS TO THE OTHER A NONEXCLUSIVE EASEMENT APPURTENANT TO PARCEL ONE AND PARCEL TWO RESPECTIVELY FOR DRIVING PARKING AND WALKING ON THE AREA DESIGNATED AS THE PARKING LOT AND ADJOINING WALKS AND DRIVEWAYS. THIS EASEMENT SHALL EXTEND TO ALL ACTIVITY NORMALLY ASSOCIATED WITH A SHARED PARKING LOT FACILITY INCLUDING BUT NOT LIMITED TO:

- (1) AN UNRESTRICTED EASEMENT FOR PEDESTRIAN TRAFFIC OVER THE ENTIRE PARKING LOT, ADJOINING WALKS AND DRIVEWAYS.
- (2) AN EASEMENT FOR INGRESS AND EGRESS FROM AND TO ALL OF THE PARKING SPACES.

THE ABOVE RECIPROCAL EASEMENT SHALL BE ENJOYED FREE OF CHARGE.

THE EASEMENT FOR DRIVING, PARKING AND WALKING SHALL EXTEND TO THE EASEMENT HOLDER AND ALL OF THE BUSINESS CONDUCTED ON THE EASEMENT HOLDER'S PARCEL AND TO THE EASEMENT HOLDER'S AND THOSE BUSINESSSES, OFFICERS, DIRECTORS, EMPLOYEES, CUSTOMERS, SUPPLIERS AND OTHER INVITEES. THE PARKING SPACES, SHALL BE USED SOLELY FOR THE TEMPORARY PARKING OF VEHICLES LICENSED TO TRAVEL ON CALIFORNIA HIGHWAYS. NO VEHICLES SHALL BE STORED IN A PARKING SPACE, AND NOTHING OTHER THAN VEHICLES SHALL BE PLACED ON A PARKING SPACE.

BY THE EASEMENTS GRANTED, THE PARTIES INTEND TO CREATE A UNIFIED PARKING LOT FACILITY TO SERVE BOTH PARCELS AS THOUGH BOTH PARCELS WERE OWNED BY ONE OWNER.

**RECIPROCAL DRAINAGE EASEMENT**

GRANTOR HEREBY RESERVES FOR THE BENEFIT OF PARCEL ONE AND PARCEL TWO AN EASEMENT FOR REASONABLE SURFACE DRAINAGE AND WATER RUN-OFF ACROSS, OVER, UNDER, AND THROUGH THOSE PORTIONS OF PARCEL ONE AND PARCEL TWO IN PARKING LOT AND ACCESS ROAD AREAS.

**PARCEL MAP NO. 3487**

**LEGAL DESCRIPTION**

BEING A DIVISION OF PARCEL 2 OF PARCEL MAP NO. 3282 RECORDED IN BOOK 33, PAGE 84, TULARE COUNTY RECORDS LOCATED IN SECTION 12, TOWNSHIP 20 SOUTH, RANGE 26 EAST, MOUNT DIABLO BASE AND MERIDIAN, IN THE CITY OF LINDSAY, COUNTY OF TULARE, STATE OF CALIFORNIA.

for  
**KEN SWANSON AND ROGER YATES**  
3434 TRUXTON AVENUE, SUITE 200  
BAKERSFIELD, CALIFORNIA 93301  
1 ( 805 ) 322 - 7447

**WAYNE F. HARRIS AND ASSOCIATES, INC.**  
101 WEST MORTON AVENUE  
PORTERVILLE, CALIFORNIA 93257  
1 ( 209 ) 781 - 2268

**EASEMENTS AFFECTING SAID PROPERTY**

1. MT. WHITNEY POWER COMPANY EASEMENT AND RIGHT OF WAY HOLDER UNDER DOCUMENT RECORDED IN BOOK 121 AT PAGE 46 OF DEEDS.
2. MT. WHITNEY POWER COMPANY EASEMENT AND RIGHT OF WAY HOLDER UNDER DOCUMENT RECORDED IN BOOK 33 AT PAGE 438 OF CONTRACTS.
3. SOUTHERN CALIFORNIA EDISON, A CORPORATION, EASEMENT AND RIGHT OF WAY HOLDER UNDER DOCUMENT RECORDED IN BOOK 3250, PAGE 711 OF OFFICIAL RECORDS.

**IMPROVEMENT CERTIFICATE**

1. THAT THE SUBDIVIDER EXTEND THE EXISTING 8" SEWER LINE, LOCATED APPROXIMATELY 185 FEET NORTH OF PARCEL 2 IN A 10' WIDE UTILITY EASEMENT WHICH RUNS PARALLEL AND ADJACENT TO THE EAST SIDE OF STATE HIGHWAY 65 FOR THE FULL LENGTH OF THE SUBJECT SITE, SOUTHERLY WITHIN SAID EASEMENT TO THE NORTH PROPERTY LINE OF PARCEL 2 PRIOR TO THE RECORDATION OF THE FINAL MAP, OR POST A BOND OR OTHER ACCEPTABLE INSTRUMENT OF CREDIT IN AN AMOUNT ESTIMATED AND DETERMINED BY THE CITY ENGINEER TO BE SUFFICIENT TO CAUSE SUCH IMPROVEMENT TO BE MADE.
2. THAT PRIOR TO THE ISSUANCE OF ANY BUILDING PERMITS ON EITHER PARCEL, THE SUBDIVIDER/DEVELOPER SHALL PROVIDE EVIDENCE TO THE SATISFACTION OF THE CITY ENGINEER OF HAVING OBTAINED AN ENCROACHMENT PERMIT FROM CALTRANS FOR NOT MORE THAN TWO (2) DRIVEWAY APPROACHES TO ACCESS THE SUBJECT PROPERTY FROM HIGHWAY 65; AND
  - A) THE ISSUANCE OF THE ENCROACHMENT PERMIT SHALL BE AT THE SOLE DISCRETION OF CALTRANS WITH RESPECT TO THE NUMBER, SIZE, LOCATION, DESIGN AND SPACING OF DRIVEWAY INCLUDING DRIVEWAYS, ROAD APPROACHES, CURBS, GUTTERS, DRAINAGE FACILITIES, ETC. WILL REQUIRE CALTRANS' APPROVAL. SUCH APPROVAL IS EXPECTED TO BE PREDICATED ON THE SUBDIVIDER/DEVELOPER BEING RESPONSIBLE FOR CERTAIN OTHER WORKS OF IMPROVEMENT WITHIN HIGHWAY RIGHT-OF-WAY AS MAY BE REQUIRED BY THE STATE, AS EVIDENCED BY CALTRANS DRAWING NO. RA-761 ON FILE IN THE OFFICE OF THE LINDSAY CITY ENGINEER.
  - B) ALL WORK PROPOSED AND REQUIRED WITHIN THE HIGHWAY RIGHT-OF-WAY UNDER THE TERMS OF AN APPROVED ENCROACHMENT PERMIT WILL BE PERFORMED TO STATE STANDARDS AND SPECIFICATIONS AT NO COST TO THE STATE OR TO THE CITY OF LINDSAY.
  - C) SHOULD CALTRANS APPROVE A SINGLE DRIVEWAY APPROACH OFF OF HIGHWAY 65 TO SERVE BOTH PARCELS, OR IF EITHER PARCEL SHOULD NOT HAVE A FULL WIDTH TWO-WAY DRIVEWAY APPROACH TO HIGHWAY 65 WHOLELY ALIGNED ON THEIR RESPECTIVE COMMON PROPERTY LINES WITH THE HIGHWAY, THEN NO BUILDING PERMIT WILL BE ISSUED FOR DEVELOPMENT ON EITHER PARCEL UNTIL SUCH TIME AS IT IS EVIDENCED TO THE SATISFACTION OF THE CITY ENGINEER THAT THE DEEDS FOR BOTH PARCELS HAVE BEEN ENCUMBERED WITH A MUTUAL RIGHT OF INGRESS, EGRESS AND MAINTENANCE EASEMENT AFFORDING VEHICULAR PASSAGE FROM ONE PARCEL TO THE OTHER WHICH CONNECTS WITH A DRIVEWAY APPROACH TO HIGHWAY 65 APPROVED BY CALTRANS.
3. THAT PRIOR TO THE ISSUANCE OF ANY BUILDING PERMITS ON EITHER PARCEL, DRAINAGE CALCULATIONS AND AN ENGINEERED DRAINAGE PLAN DESIGNED TO EVIDENCE CAPACITY AND CONVEY STORM WATER TO THE MARIPOSA POND FROM BOTH PARCELS SHALL BE PREPARED AND SUBMITTED BY THE SUBDIVIDER/DEVELOPER FOR APPROVAL BY THE CITY ENGINEER.

CENTER OF SECTION 12,  
T.22S., R.26E., M.D.B. & M.  
FD. 3/4" I.P.

EAST 1/4 CORNER OF SEC. 12,  
T.22S., R.26E., M.D.B. & M.  
FD. 2" I.P.

HERMOSA STREET

SHEET TWO OF TWO SHEETS

2-18-87

RECORDING REQUESTED BY:

City of Lindsay

AND WHEN RECORDED MAIL TO:

City Clerk  
City of Lindsay  
251 East Honolulu Street  
Lindsay, CA 93247

---

## CERTIFICATE OF CORRECTION

I, Dale G. Mell, PLS 4823, being duly sworn, deposes and says:

That the following corrections or additions to the map of Parcel Map No. 3487, as filed in Volume 35 of Maps, Page 89 in the office of the Recorder, County of Tulare, have been made by me in accordance with Section 66472.1 of the Subdivision Map Act:

The condition and notation shown on the map “30 foot wide building setback line located along the easterly line of parcels one and two of this map” are no longer appropriate and therefore deleted.

Names of property owners: Maninder S. Sandhu  
Dyland Inn LLC

---

Dale G. Mell, PLS 4823

Certificate of City Engineer

This is to certify that the above certificate of correction has been examined for compliance with Section 66469 of the Subdivision Map Act.

Dated: \_\_\_\_\_

Signature: \_\_\_\_\_

Jim S. Winton, R.C.E. 16094  
City Engineer

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RESOLUTION CC 86-62

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LINDSAY APPROVING TENTATIVE PARCEL MAP APPLICATION NO. 86-18, SUBMITTED BY KEN SWANSON AND ROGER YATES, 3434 TRUXTUN AVENUE-SUITE 200, BAKERSFIELD, CALIFORNIA.

At a regularly scheduled meeting of the City Council of the City of Lindsay, held on December 1, 1986, at the hour of 7:00 p.m. of said day, in the City Council Chambers, City Hall, Lindsay, California, upon motion by Councilperson Linville, seconded by Councilperson Lencioni, the following resolution was adopted:

THAT WHEREAS, the Lindsay City Council reviewed and considered Tentative Parcel Map (TPM) Application No. 86-18, said application having been submitted for approval to divide a 3.13 acre parcel in the City of Lindsay, located east and adjacent to State Highway 65 between Hermosa Street and Mariposa Street, into two parcels of 2.21 acres and 0.92 acres respectively; and

WHEREAS, the City Council received testimony during the scheduled public hearing from all interested parties relative to the TPM Application; and

WHEREAS, the City Council hereby certifies that it has reviewed and considered the information contained in the mitigated Negative Declaration for said TPM Application prior to taking action on the project; and

WHEREAS, the City Council, after considering all the evidence presented, determined the following findings were relevant in evaluating the TPM Application:

1. The Land Use Element of the Lindsay General Plan designates this site for Highway Commercial development.

1           2. Existing zoning on the subject site is CH (Highway Com-  
2 mercial). Information submitted with the tentative map indicates a  
3 51-unit Motel and a Restaurant are proposed to be developed on the  
4 site.

5           3. A major policy and objective of the Circulation Element  
6 of the Lindsay General Plan is the designation and implementation  
7 of a planned frontage road east of State Highway 65 to provide ade-  
8 quate future access to commercial uses locating in close proximity  
9 to the Highway.

10           4. The project is proposed to be accessed, on an interim  
11 basis, directly from State Highway 65.

12           5. Domestic water and sewage disposal facilities are avail-  
13 able to the site, and are adequate to serve the uses proposed  
14 thereon.

15           6. A storm water retention basin is located in close proximi-  
16 ty to the site, and the subdivider/developer will be responsible  
17 for evidencing adequate capacity therein, and designing acceptable  
18 conveyance facilities thereto, prior to commencing proposed project.

19           7. The TPM Application was filed pursuant to applicable  
20 regulations and provisions contained in the State Subdivision Map  
21 Act and the Subdivision Ordinance of the City of Lindsay, and with  
22 the exception of Section 19.7.1500 of Local Ordinance 341, and  
23 Government Code Section 66473.5, is consistent with all other  
24 applicable sections of the Subdivision Map Act and Lindsay Subdi-  
25 vision Ordinance.

26           8. The City Council acknowledged that the Environmental  
27 Review Officer of the City of Lindsay accepted and found to be  
28

1 adequate for public review under the provisions of the California  
2 Environmental Quality Act of 1970, as amended, an Environmental  
3 Assessment finding that said TPM Application will not have a  
4 significant effect on the environment pursuant to incorporation of  
5 mitigation measures identified in the Initial Environmental Study,  
6 and the City Council adopted Resolution CC 86-61 approving the  
7 Negative Declaration prepared for the project.

8           NOW, THEREFORE, BE IT RESOLVED AS FOLLOWS: That the City  
9 Council of the City of Lindsay does hereby approve Tentative Parcel  
10 Map Application No. 86-18, subject to the following conditions:

11           1. That the subdivider, and his designated agents, shall  
12 adhere to all applicable provisions and requirements of the Subdi-  
13 vision Map Act and Ordinances of the City of Lindsay in the prepa-  
14 ration of the Final Parcel Map and any required improvement  
15 drawings attendant thereto.

16           2. That prior to the recordation of the Final Parcel Map,  
17 the subdivider shall cause a preliminary soils report to be pre-  
18 pared and presented to the City Engineer for Parcel No. 2, unless,  
19 in the opinion of the City Engineer, sufficient soils information  
20 on the site is already on file with the City.

21           3. That prior to the issuance of any Building Permits on  
22 either parcel, drainage calculations and an engineered drainage  
23 plan designed to evidence capacity and convey storm water to the  
24 Mariposa Pond from both parcels shall be prepared and submitted by  
25 the subdivider/developer for approval by the City Engineer, and  
26 such condition shall be noticed on the Final Map (Highway drainage  
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1 shall be in accordance with Caltrans plans and requirements - See  
2 Condition No. 5).

3  
4 4. That the Subdivider extend the existing 8" sewer line,  
5 located approximately 185' North of Parcel 2 in a 10' wide utility  
6 easement which runs parallel and adjacent to the east side of State  
7 Highway 65 for the full length of the subject site, southerly  
8 within said easement to the North Property Line of Parcel 2 prior  
9 to the recordation of the Final Map, or post a bond or other  
10 acceptable instrument of credit in an amount estimated and deter-  
11 mined by the City Engineer to be sufficient to cause such improve-  
12 ment to be made.

13 5. That prior to the issuance of any Building Permits on  
14 either parcel, the subdivider/developer shall provide evidence to  
15 the satisfaction of the City Engineer of having obtained an  
16 Encroachment Permit from Caltrans for not more than two (2) drive-  
17 way approaches to access the subject property directly from State  
18 Highway 65; and

19 a) The issuance of the Encroachment Permit shall be at  
20 the sole discretion of Caltrans with respect to the number,  
21 size, location, design and spacing of driveway approaches,  
22 and, if approved, all work proposed within the Highway  
23 right-of-way, including driveways, road approaches, curbs,  
24 gutters, drainage facilities, etc., will require Caltrans'  
25 approval. Such approval is expected to be predicated on the  
26 subdivider/developer being responsible for certain other  
27 works of improvement within the Highway right-of-way as may  
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be required by the State, as evidenced by Caltrans Drawing No. RA-761 on file in the office of the Lindsay City Engineer.

b) All work proposed and required within the Highway right-of-way under the terms of an approved Encroachment Permit will be performed to State Standards and Specifications at no cost to the State or to the City of Lindsay.

c) Should Caltrans approve a single driveway approach off of Highway 65 to serve both parcels, or if either parcel should not have a full-width two-way driveway approach to Highway 65 wholly aligned on their respective common property lines with the Highway, then no Building Permit will be issued for development on either parcel until such time as it is evidenced to the satisfaction of the City Engineer that the Deeds for both parcels have been encumbered with a mutual right of ingress, egress and maintenance easement affording vehicular passage from one parcel to the other which connects with a driveway approach to Highway 65 approved by Caltrans.

d) Condition 5, and subparagraphs a, b and c thereof, shall be noted on the Final Map unless the Subdivider has complied with same prior to the recordation of the Final Map.

6. That when recorded, the Final Parcel Map have delineated thereon a 30' building setback line westerly, parallel and contiguous to the full length of the east property lines of Parcel 1 and Parcel 2. Said 30' wide building setback will reserve an area, free from structural improvements, all or part of which could

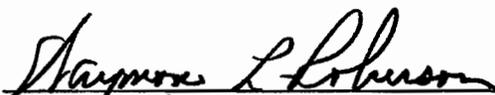
1 be acquired with other frontage road street acquisition if and when.  
2 necessary.

3  
4 AYES : Linville, Lencioni, Hendricks, Blue, Roberson

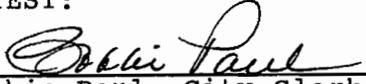
5 NOES : NONE.

6 ABSENT : NONE.

7 ABSTAIN: NONE.  
8

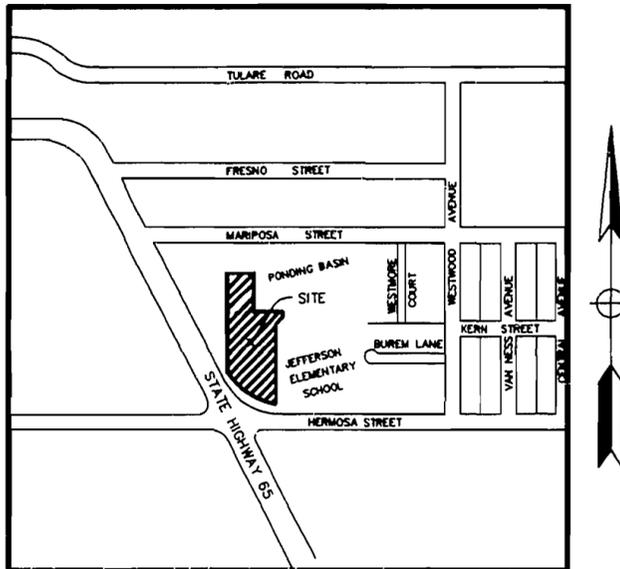
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Waymon Roberson, Mayor

11 ATTEST:

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13 Bobbie Paul, City Clerk

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**PREPARED BY:**  
**CEI ENGINEERING ASSOCIATES, INC**  
 ENGINEERS PLANNERS SURVEYORS  
 4270 W. Richert Ave., #109 (209) 277-7424  
 Fresno, CA 93722 FAX (209) 277-7429



**Vicinity Map**  
 Not to Scale

**PARCEL MAP NO. 4310**

BEING A DIVISION OF A PORTION OF BLOCK 63 OF THE OFFICIAL MAP OF THE CITY OF LINDSAY AS RECORDED IN BOOK 17 OF MAPS, PAGE 57, T.C.R., SITUATED IN THE SE 1/4 OF THE NE 1/4 OF SECTION 12, T. 20 S., R. 26 E., M.D.B. & M., IN THE CITY OF LINDSAY, COUNTY OF TULARE, STATE OF CALIFORNIA  
 CONSISTING OF TWO SHEETS

**OWNER'S STATEMENT**

WE HEREBY STATE THAT WE ARE THE OWNERS OF OR HAVE SOME RIGHT, TITLE OR INTEREST IN AND TO THE REAL PROPERTY SHOWN ON THIS PARCEL MAP AND THAT WE ARE THE ONLY PEOPLE WHOSE CONSENT IS NECESSARY FOR THE MAKING AND FILING OF SAID PARCEL MAP AS SHOWN WITHIN THE BLUE BORDER LINES HEREOF

*Larry E. Stone*  
 LARRY E. STONE  
*Carol E. Stone*  
 CAROL E. STONE  
*David M. Stone*  
 DAVID M. STONE

**COUNTY ADMINISTRATIVE OFFICERS STATEMENT**

STATE OF CALIFORNIA  
 COUNTY OF TULARE  
 I, THOMAS F. CAMPANELLA, COUNTY ADMINISTRATIVE OFFICER/CLERK OF THE BOARD OF SUPERVISORS OF THE COUNTY OF TULARE, DO HEREBY STATE THAT SAID BOARD OF SUPERVISORS HAS APPROVED THE PROVISIONS MADE FOR THE PAYMENT OF TAXES AS PROVIDED IN DIVISION 2 OF TITLE 7 OF THE GOVERNMENT CODE OF THE STATE OF CALIFORNIA.

DATED THIS 30<sup>th</sup> DAY OF May, 1997

THOMAS F. CAMPANELLA  
 COUNTY ADMINISTRATIVE OFFICER/CLERK  
 BOARD OF SUPERVISORS

BY: *Sandra M. Conal*  
 DEPUTY CLERK



State of California )  
 County of Tulare )  
 On \_\_\_\_\_ before me,  
 personally appeared \_\_\_\_\_  
 personally known to me (or proved to me on the basis of satisfactory evidence) to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), (or the entity upon behalf of which the person(s) acted,) executed the instrument.  
 WITNESS my hand

Name \_\_\_\_\_  
 Signature \_\_\_\_\_  
 Commission Expires \_\_\_\_\_  
 County \_\_\_\_\_

**PLANNING DIRECTOR'S STATEMENT**

I HEREBY STATE THAT THIS FINAL PARCEL MAP CONFORMS TO THE APPROVED TENTATIVE MAP NO. 97-04.

DATED THIS 20<sup>th</sup> DAY OF May, 1997

*Scott B. Townsend*  
 SCOTT B. TOWNSEND, COMMUNITY DEVELOPMENT DIRECTOR

State of California )  
 County of Tulare )  
 On \_\_\_\_\_ before me,  
 personally appeared \_\_\_\_\_  
 personally known to me (or proved to me on the basis of satisfactory evidence) to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), (or the entity upon behalf of which the person(s) acted,) executed the instrument.  
 WITNESS my hand

Name \_\_\_\_\_  
 Signature \_\_\_\_\_  
 Commission Expires \_\_\_\_\_  
 County \_\_\_\_\_

State of California )  
 County of Tulare )  
 On MAY 28, 1997 before me,  
*Helen Wadd*  
 personally appeared *Larry E. Stone, Carol E. Stone & David M. Stone*  
 personally known to me (or proved to me on the basis of satisfactory evidence) to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), (or the entity upon behalf of which the person(s) acted,) executed the instrument.  
 WITNESS my hand

Name *Helen WADD*  
 Signature *Helen Wadd*  
 Commission Expires *MAY 31, 1998*  
 County *Tulare*

**CITY ENGINEER'S STATEMENT**

THIS MAP CONFORMS WITH REQUIREMENTS OF THE SUBDIVISION MAP ACT AND LOCAL ORDINANCE

DATED THIS 20<sup>th</sup> DAY OF May, 1997

*Wm R Drennen*  
 WM. R. DRENNEN, CITY ENGINEER RCE 23815



**SURVEYOR'S STATEMENT**

THIS MAP WAS PREPARED BY ME OR UNDER MY DIRECTION AND IS BASED UPON A FIELD SURVEY IN CONFORMANCE WITH THE REQUIREMENTS OF THE SUBDIVISION MAP ACT AND LOCAL ORDINANCES AT THE REQUEST OF SYDRAN SERVICES, INC ON FEBRUARY 10, 1997 I HEREBY STATE THAT THIS PARCEL MAP SUBSTANTIALLY CONFORMS TO THE APPROVED OR CONDITIONALLY APPROVED TENTATIVE MAP, IF ANY ALL MONUMENTS ARE OF THE CHARACTER AND OCCUPY THE POSITIONS INDICATED AND ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED

*Michael E. Long* 5/23/97  
 MICHAEL E. LONG PLS 6815



**RECORDER'S STATEMENT**

DOCUMENT NO 97-036554 FEE PAID \$16.00  
 FILED THIS 30<sup>th</sup> DAY OF May, 1997, AT 2:03 PM IN BOOK 44 OF PARCEL MAPS, AT PAGE 14, AT THE REQUEST OF SYDRAN SERVICES, INC

GREGORY B. HARDCASTLE  
 TULARE COUNTY ASSESSOR/CLERK RECORDER

BY: *Gregory B. Hardcastle*  
 DEPUTY

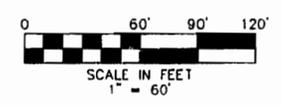
WESTWOOD

AVENUE

TPM 97-04

PARCEL MAP NO. 4310

BEING A DIVISION OF A PORTION OF BLOCK 63 OF THE OFFICIAL MAP OF THE CITY OF LINDSAY AS RECORDED IN BOOK 17 OF MAPS, AT PAGE 57, T.C.R., SITUATED IN THE SE 1/4 OF THE NE 1/4 OF SECTION 12, T 20 S., R 26 E., M.D.B. & M., IN THE CITY OF LINDSAY, COUNTY OF TULARE, STATE OF CALIFORNIA



THIS PROPERTY IS SUBJECT TO THE FOLLOWING EASEMENT

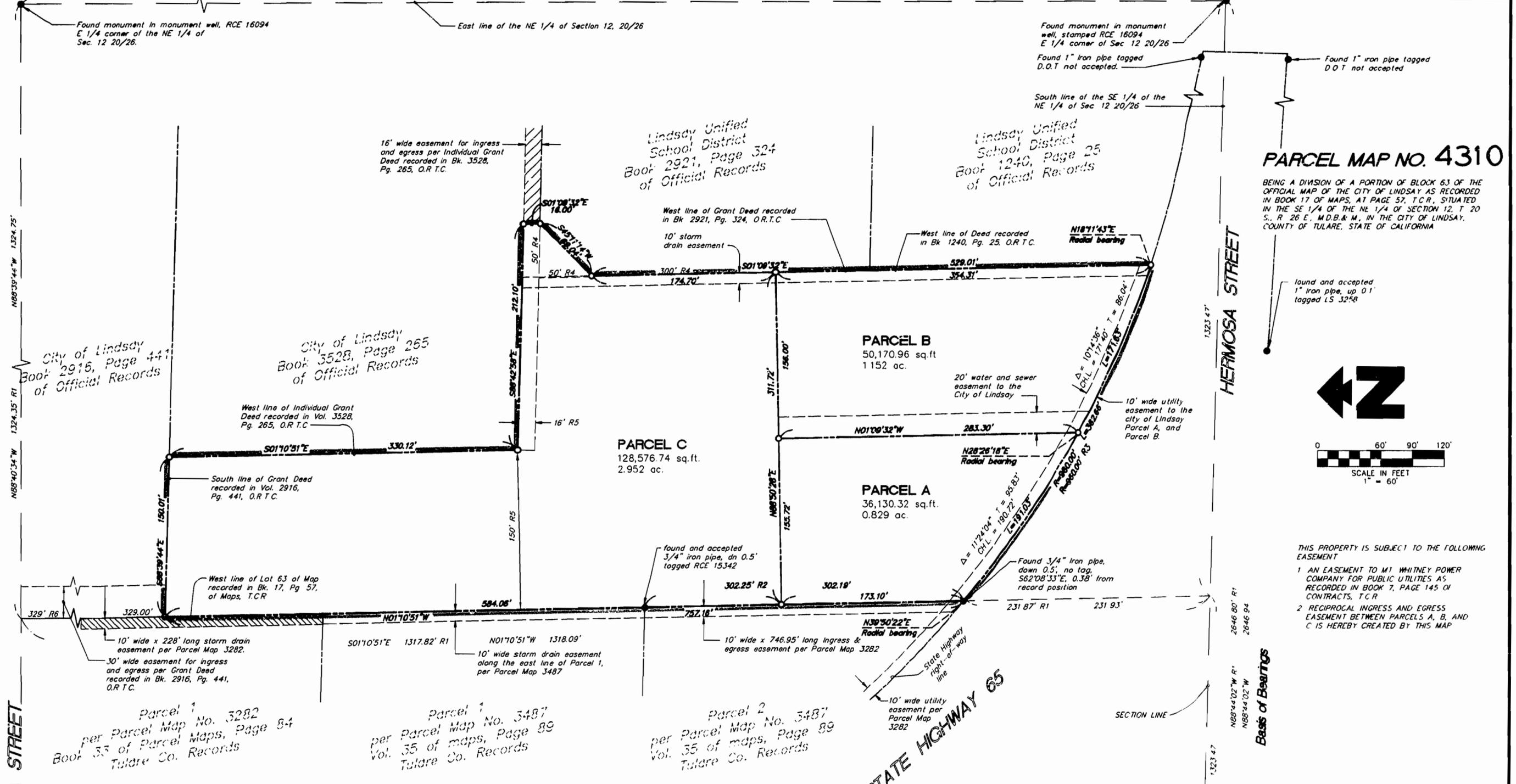
- 1 AN EASEMENT TO M1 WHITNEY POWER COMPANY FOR PUBLIC UTILITIES AS RECORDED IN BOOK 7, PAGE 145 OF CONTRACTS, T.C.R.
- 2 RECIPROCAL INGRESS AND EGRESS EASEMENT BETWEEN PARCELS A, B, AND C IS HEREBY CREATED BY THIS MAP

**CEI ENGINEERING ASSOCIATES, INC**  
 ENGINEERS • PLANNERS • SURVEYORS  
 4270 W. Richert Ave., #109 (209) 277-7424  
 Fresno, CA 93722 FAX (209) 277-7429  
 DWG NAME 2577PM1 JOB NO 12577 101  
 DATE 05-23 97

MARIPOSA STREET

HERMOSA STREET

STATE HIGHWAY 65



LEGEND

- FOUND AND ACCEPTED MONUMENT AS DESCRIBED, UNLESS OTHERWISE NOTED
- SET 3/4" X 30" IRON PIPE, DOWN 6", TAGGED PLS 6815
- R1 RECORD INFORMATION PER PARCEL MAP NO 3282, BK 33, OF PARCEL MAPS, PG 84, T.C.R
- R2 RECORD INFORMATION PER PARCEL MAP NO 3487, BK 35, OF MAPS, PG 89, T.C.R
- R3 RECORD INFORMATION PER CALTRANS MAP: 6-TUL-65-29 63 SHt 70
- R4 RECORD INFORMATION PER GRANT DEED, BK 2921, PG 324, O.R.T.C
- R5 RECORD INFORMATION PER INDIVIDUAL GRANT DEED, BK 3528, PG. 265, O.R.T.C
- R6 RECORD INFORMATION PER GRANT DEED, BK 2916, PG 441, O.R.T.C
- INDICATES SUBDIVISION BOUNDARY
- ALL BEARINGS SHOWN ARE IN DEGREES, MINUTES, AND SECONDS
- ALL DISTANCES SHOWN ARE IN FEET AND DECIMALS, THEREOF

BASIS OF BEARINGS

THE SOUTH LINE OF THE NORTHEAST QUARTER OF SECTION 12, TOWNSHIP 20 SOUTH, RANGE 26 EAST, M.D.B & M SAID LINE IS TAKEN TO BEAR N88°44'02"W

**RESOLUTION NO. 15-34**  
**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LINDSAY**  
**APPROVING THE MODIFICATION OF PARCEL MAP 3487 (APN'S: 199-280-001**  
**AND 199-280-002)**

At a regular meeting of the City Council of the City of Lindsay, held July 14, 2015, at the hour of 6:00 p.m. in the Council Chambers at City Hall, Lindsay, California 93247, the following resolution was adopted:

**THAT WHEREAS**, a Certificate of Correction to modify Parcel Map 3487 (APN's 199-280-001 and 199-280-002) was filed pursuant to the regulations contained in the Subdivision Map Act and Ordinance 549; and

**WHEREAS**, the Certificate of Correction specifically states that the condition and notation shown on Parcel Map 3487 "30 foot wide building setback line located along the easterly line of parcels one and two of this map" are no longer appropriate and therefore deleted; and

**WHEREAS**, the City Council of the City of Lindsay did hold a noticed public hearing before said Council on July 14, 2015; and

**WHEREAS**, planning staff has prepared necessary investigations and prepared a staff report of information bearing upon the modification of Parcel Map 3487; and

**WHEREAS**, the modification of Parcel Map 3487 is Categorically Exempt from the California Environmental Quality Act per Title 14 CCR, Chapter 3, Article 19, Section 15305 'Minor Alterations in Land Use Limitations'.

**NOW, THEREFORE, BE IT RESOLVED**, that the City Council of the City of Lindsay finds the proposed modification of Parcel Map 3487 is consistent with the provisions of the Subdivision Map Act, Section 66472.1 and Ordinance No. 549, which govern map modifications; and

**BE IT FURTHER RESOLVED**, that the City Council of the City of Lindsay hereby approves the modification of Parcel Map 3487 in accordance with the following findings:

**WHEREAS**, there are changes in circumstances which make any or all of the conditions of a recorded map no longer appropriate or necessary.

**WHEREAS**, the modifications do not impose any additional burden on the fee owners of the real property.

**WHEREAS**, the modifications do not alter any right, title, or interest in the real property reflected on the recorded map.

**WHEREAS**, the map amendments are consistent with the City of Lindsay General Plan Goals and Policies.

**WHEREAS**, the map, as modified, conforms to the provisions of Section 17.24.160 of the City of Lindsay Subdivision Ordinance and Section 65451 of the Subdivision Map Act.

**PASSED, APPROVED AND ADOPTED** by the City Council of the City of Lindsay this 14th day of July, 2015.

ATTEST:

CITY COUNCIL OF THE CITY OF LINDSAY

\_\_\_\_\_  
Carmela Wilson, City Clerk

\_\_\_\_\_  
Ramona Villarreal-Padilla, Mayor

## PUBLIC HEARING PROCEDURES

The following rules shall apply:

1. OPEN the public hearing.
2. PROPOSERS (those in favor). The Council may ask questions of the proponents and they may respond.
3. OPPOSERS (those against). The Council may ask questions of the opponents and they may respond.
4. REBUTTAL each side.
5. FURTHER QUESTIONS from Council, but the parties may not engage in further debate.
6. CLOSE the public hearing.
7. COUNCIL DISCUSSION.
8. MOTION ( if necessary).
9. COUNCIL VOTE.

**CITY OF LINDSAY  
STAFF REPORT  
SITE PLAN REVIEW No. 15-03  
(New Subway Restaurant)  
July 14, 2015**

**GENERAL INFORMATION**

- |                      |   |
|----------------------|---|
| 1. Applicant:        | Maninder S. Sandhu<br>1163 E. Pinehurst Ave.<br>Fresno, CA 93730                  |
| 2. Requested Action: | Site Plan Review approval to construct a new Subway Restaurant with Drive-Through |
| 3. Location:         | Northeast corner of Hwy 65 and Hermosa Street (APN: 199-280-002)                  |

**PROJECT DESCRIPTION**

Site Plan Review No. 15-03 is a request by Maninder S. Sandhu to construct a new Subway Restaurant at the NE corner of Hwy 65 and Hermosa, adjacent to the Country Waffle Restaurant on the southernmost portion of a .95 acre site (APN: 199-280-002). Specifically the project would be located on the south parking area and lawn. The project would share site access and parking with the Country Waffle and the Super 8 Motel and would include a drive-through pickup window at the Subway. Mr. Sandhu owns the Country Waffle property and the existing Lindsay Subway Restaurant. An aerial photo and site drawings with elevations are contained within the attached CEQA documents.

**SITE BACKGROUND**

**Background:** The project site is part of the Country Waffle property, created in 1987 by Parcel Map 3487, which split a 3.14 acre lot into two parcels to accommodate a motel (currently, the Super 8 Motel) to the north and a restaurant (currently, the Country Waffle Restaurant) to the south. Parcel 1 (Super 8) is 2.19 acres and Parcel 2 (Country Waffle) is .95 acres in area. Parcel Map 3487 included a non-exclusive easement for driving, parking, and walking in areas designated as the parking lot and adjoining driveways and sidewalks as well as a reciprocal drainage easement. Condition six of City of Lindsay resolution CC 86-62 required that Parcel Map 3487 additionally created a thirty-foot wide setback line located along the easterly line of parcels 1 and 2 to “reserve an area, free from structural improvement, all or part of which could be acquired with other frontage road street acquisition if and when necessary.” This appears to have been intended to reserve an area for a one-half street right-of-way (ROW) along the eastern property lines of the parcels created by Parcel Map 3487.

Parcel Map 4310 was created in 1997 to subdivide the adjacent property to the east; however, no similar condition was placed on this map; therefore, a proper street cannot be created in this area without new and independent actions of condemnation on the part of City Council. It should be noted that no street was planned for this area on the 1989 General Plan map, which was developed following the CC 86-62 resolution requirement.

Surrounding land uses for the project site include:

- North: Super 8 Motel (highway commercial use)
- South: Hermosa Street and to the south of Hermosa Street, the Olivewood Plaza
- East: Burger King (highway commercial use) and vacant land
- West: Highway 65 and to the west of Hwy 65, vacant land

### COMPATIBILITY WITH EXISTING ZONING, PLANS AND POLICIES

**Zoning and Land Use:** The proposed project is consistent with the policies, objectives, and standards of the Lindsay General Plan and Zoning Ordinance. Specifically, a restaurant is a permitted use within the highway commercial (CH) zoning district with site plan review approval.

**Circulation:** The proposed site would take access from two possible commercial drive approaches along Highway 65, north of the Country Waffle Restaurant and adjacent to the Super 8 Motel. Drive-through traffic would be configured to flow in a counter-clockwise direction, looping south around the building to the pickup window on the east side of the restaurant. This would allow for the efficient queuing of vehicles around the perimeter of the restaurant. Following food purchase, vehicles would return to the main shared parking area and drive approaches. The drive-through turning radius was professionally engineered to accommodate a range of common passenger vehicles. Seven dedicated parking spaces would be provided along the north side of the restaurant, with one being handicapped accessible.

**Public Services:** The site is within a convenient response time of public safety services. Existing water supply and conveyance facilities would provide adequate fire suppression capabilities. The project site is currently served by all City utilities.

### PROJECT EVALUATION

The proposed project would convert an underutilized private lawn and adjacent parking area into a new highway commercial business opportunity with drive-through access at the NE corner of Hwy 65 and Hermosa Street. Shared ingress and egress to Highway 65 along with adequate dedicated and shared parking are available to the project site.

Approval of resolution 15-34 modified Parcel Map 3487 by removing the thirty-foot building setback requirement. Zoning Ordinance Section 18.10.070.E. governs setbacks within commercial zoning districts and allows that no side yard or rear yard setback is required when abutting similarly zoned properties. This project site abuts only other highway commercial (CH) properties; therefore, no side or rear yard setback

is required.

Zoning Ordinance Section 18.18.050 lists several findings that are needed for approving Site Plan Review applications. In sum, this includes findings that the project complies with all applicable codes and would not adversely impact public health, safety, or welfare. Staff finds that the proposed site plan is consistent with the requirements of the Zoning Ordinance. Staff recommends that the project be developed as proposed.

## **ENVIRONMENTAL REVIEW**

An initial study was performed and a draft Mitigated Negative Declaration has been prepared (see attached). Proposed mitigation measures are as follow:

- **Aesthetics:** The project would incorporate standard light shielding measures for street light fixtures to mitigate any potential adverse glare impacts.
- **Air Quality:** The project would be subject to all applicable mandatory air pollution control measures of the San Joaquin Valley Unified Air Pollution Control District in effect at time of development, including, but not limited to: Regulation VIII (Fugitive PM10 Prohibitions), Regulation VIII (Rules 8011-8081), Rule 4102 (Nuisance), 4103 (Open Burning), Rule 4601 (Architectural Coatings), Rule 4641 (Cutback, Slow Cure, and Emulsified Asphalt, Paving and Maintenance Operations), Rule 4901 (Wood Burning Fire Places and Wood Burning Heaters), and Rule 9510 (Indirect Source Review). The developer would specifically demonstrate compliance with San Joaquin Valley Air Pollution Control District Rule 9510 (Indirect Source Review), including payment of all applicable fees, prior to the issuance of the first building permit.
- **Cultural Resources:** Pursuant to CEQA Guidelines 15064.5 (f), provisions for historical or unique archaeological resources accidentally discovered during construction would be instituted. Therefore, in the event that any prehistoric or historic subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources would be halted and a qualified archaeologist or paleontologist shall be contacted to assess the significance of the find. If any find is determined to be significant, project proponents and the qualified archaeologist and/or paleontologist would meet to determine the appropriate avoidance measures or other appropriate mitigation. All significant cultural materials recovered would be subject to scientific analysis, professional museum curation, and a report prepared by the qualified archaeologist according to current professional standards. If the discovery includes human remains, CEQA Guidelines 15064.5 (e)(1) would be followed.
- **Noise:** High noise levels resulting from construction activities would be limited to the hours of 6:00 a.m. to 10:00 p.m., including weekends and holidays.

A CEQA Notice of Determination has been prepared and will be submitted pending adoption of the Mitigated Negative Declaration by Council (draft attached).

## **RECOMMENDATION**

Staff recommends that the City Council approves Site Plan Review No. 15-03, based on the findings and subject to the conditions found below and in the attached draft

resolution.

- Development would be in substantial conformance to the submitted site plan and building renderings.
- An ADA compliant, six-foot-wide sidewalk would be required along the entire Hwy 65 property frontage, including handicapped returns at drive approaches, as appropriate.
- Any signage would require a separate review and approval by the planning department. A sign permit would be required for all new commercial signs, prior to sign installation. All permanent and temporary signs, banners, and/or other special promotional signage would comply with all applicable Zoning Ordinance sign standards
- A landscape and irrigation plan would be submitted to the city as part of the first building permit submittal. Said plan would conform to the California Model Landscape Ordinance. Required landscape improvements would be installed prior to the issuance of a certificate of occupancy.
- The property owner would be responsible to maintain all required landscaping areas. This specifically includes replacement of damaged, removed, or dead plants, proper irrigation, fertilizing, pruning, mowing, and weeding as necessary to insure high quality landscaping appearances at all times.
- All existing and proposed mechanical equipment (ground or roof mounted) would be screened from public view. Roof-mounted mechanical equipment would be screened by design, with screening measures clearly indicated in the building permit application package.
- The building permit application would include a soils report. Paving sections would be designed based on the results of the soils report, to City Standards and acceptance of the director of city services.
- The building permit application would include a lighting plan for all exterior lighting. Exterior lighting would be positioned and shielded as necessary to prevent light or glare from impinging on adjacent properties and roads.
- A site grading and drainage plan would be submitted to the city for review and approval.
- City and State recycling requirements would be met.
- A city-standard split-faced masonry trash and recycling enclosure with screened gates would be constructed with adequate capacity to accommodate the waste generated by any and all businesses located on the site, to the satisfaction of the director of city services.
- The CEQA mitigation measures described above would be followed.
- Development would be coordinated with Caltrans, including but not limited to the following Caltrans requirements and recommendations:
  - *No new direct access from the proposed development to SR 65 shall be allowed.*
  - *If a driveway is to be shared by two or more property owners, an access easement (or an agreement acceptable to the State) needs to be executed between the parties and submitted to the Encroachment Permit office before a permit is issued for any work in the State right of way.*

- *Any existing or proposed driveways accessing State right-of-way must meet current State standards.*
- *Caltrans recommends that a minimum six feet wide sidewalk be installed along the SR 65 to provide access for pedestrians and be constructed to meet current ADA standards or other applicable State or Federal accessibility and safety requirements.*
- *A sidewalk and landscape maintenance agreement will be required.*
- *An encroachment permit must be obtained [from Caltrans] for all proposed activities for placement of encroachments within, under or over the State highway rights-of-way. Activity and work planned in the State right-of-way shall be performed to State standards and specifications, at no cost to the State. Engineering plans, calculations, specifications, and reports (documents) shall be stamped and signed by a licensed Engineer or Architect. Engineering documents for encroachment permit activity and work in the State right-of-way may be submitted using English Units. The Permit Department and the Environmental Planning Branch will review and approve the activity and work in the State right-of-way before an encroachment permit is issued. Encroachment permits will be issued in accordance with Streets and Highway Codes, Section 671.5, "Time Limitations." Encroachment permits do not run with the land. A change of ownership requires a new permit application. Only the legal property owner or his/her authorized agent can pursue obtaining an encroachment permit. Please call the Caltrans Encroachment Permit Office- District 6: 1352 W. Olive, Fresno, CA 93778, at (559) 488-4058.*
- *Advertising signs within the immediate area outside the State right-of-way need to be cleared through the Caltrans Division of Traffic Operations, Office of Outdoor Advertising. The project proponent must construct and maintain the advertising signs without access to the State Routes. Please contact the Outdoor Advertising Program, P.O. Box 942874, MS-36, Sacramento, CA 94274-0001, Phone (916) 654-6473, FAX (916) 651-9359 for additional information or to obtain a sign permit application. Additional information on Caltrans Outdoor Advertising Permit requirements may also be found on the Internet at [www.dot.ca.gov/hq/oda](http://www.dot.ca.gov/hq/oda).*
- *Any proposed landscaping plans shall meet current standards as determined by the District Landscape Architect. Proposed landscaping needs to be low growing, less than two feet in height, due to sight distance concerns. All features of landscaping shall be evaluated for type, location and site visibility conflicts during the encroachment review process. All permits for landscaping in conventional highway right-of-way must be accompanied by a "District" approved maintenance agreement obligating a local agency or the permittee to maintaining the landscaping. Said maintenance agreement must accompany and be approved prior to issuance of the landscape permit. Proposed landscape projects in access control rights-of-way require an exception process, and approval is subject to the Headquarters Departmental approval process.*

- *Dust control measures shall be implemented on the site in a manner to prevent dust from entering the State right-of-way.*
- *No water from the proposed project shall flow into the State right-of-way without approval from the District Hydraulic Engineer.*
- *Caltrans is currently working on a re-alignment project for SR 65. The project is currently in the Project Approval & Environmental Document Phase. The proposed re-alignment is to the west of the current SR 65. An alternative design that is being reviewed proposes the realignment beginning at Lindmore Street, extending north and ending at Avenue 300, just north of the SR 65/SR 198 interchange. Once this project is completed the current SR 65 right-of-way will be relinquished to the City.*

## **ATTACHMENTS**

- Draft Resolution 15-12
- CEQA Initial Study/Mitigated Negative Declaration, including Aerial Photo, Site Plan, Elevation Drawings, CalEEMod modeling data and Caltrans letter

**RESOLUTION NO. 15-12**

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LINDSAY APPROVING SITE PLAN REVIEW 15-03, NEW SUBWAY RESTAURANT PROJECT AND ACCEPTING A MITIGATED NEGATIVE DECLARATION.**

At a regular meeting of the City Council of the City of Lindsay, held July 14, 2015, at the hour of 6:00 p.m. in the Council Chambers at City Hall, Lindsay, California 93247, the following resolution was adopted:

**THAT WHEREAS**, Site Plan Review No. 15-03, the New Subway Restaurant Project and its accompanying mitigated negative declaration were filed pursuant to the regulations contained in Ordinance No. 437, the Zoning Ordinance of the City of Lindsay; and

**WHEREAS**, the City Council of the City of Lindsay did hold a noticed public hearing before said Council on April 21, 2015; and

**WHEREAS**, more time was needed to address a building setback requirement identified on Parcel Map 3487, which governs site development of the property in question; and

**WHEREAS**, the public hearing was continued until on or after June 26, 2015.

**NOW, THEREFORE BE IT RESOLVED**, that approval of City of Lindsay Resolution 15-34 removed the Parcel Map 3487 building setback requirement and that current zoning code setbacks shall apply to the properties subject to said map.

**THAT WHEREAS**, planning staff has prepared necessary investigations and prepared a staff report of information bearing upon Site Plan Review 15-03, the New Subway Restaurant project; and

**WHEREAS**, an initial study was conducted consistent with the provisions of the California Environmental Quality Act (CEQA). On the basis of the initial study the City of Lindsay has determined that the proposed project would not result in potential significant impacts on the environment. A draft mitigated negative declaration (finding of no significant impact) has been prepared. The draft mitigated negative declaration has been available for public review for over twenty days at the City of Lindsay Planning and Economic Development Department, 251 E. Honolulu Street, Lindsay, California, and on the City of Lindsay website, at <http://www.lindsay.ca.us/documents/Planning/EnvironmentalDocs.htm>.

**NOW, THEREFORE, BE IT RESOLVED**, that the City of Lindsay hereby accepts the mitigated negative declaration and instructs staff to file a notice of determination with the County.

**NOW, THEREFORE, BE IT FURTHER RESOLVED**, that the City Council of the City of Lindsay finds that the proposed project is consistent with the provisions of the City of Lindsay Zoning Ordinance (Municipal Code Title 18).

**BE IT FURTHER RESOLVED**, that the City Council of the City of Lindsay hereby approves Site Plan review No. 15-03, New Subway Restaurant Project subject to the following conditions:

**SECTION 1.** That development shall be in substantial conformance to the submitted site plan and building renderings.

**SECTION 2.** That an ADA compliant, six-foot-wide sidewalk shall be required along the entire Hwy 65 property frontage, including handicapped returns at drive approaches, as appropriate.

**SECTION 3.** That all signs shall require a separate review and approval by the planning department. A sign permit shall be required for all new commercial signs, prior to sign installation. All permanent and temporary signs, banners, and/or other special promotional signage shall comply with all applicable Zoning Ordinance sign standards.

**SECTION 4.** That a landscape and irrigation plan shall be submitted to the city as part of the first building permit submittal. Said plan shall conform to the California Model Landscape Ordinance. Required landscape improvements shall be installed prior to the issuance of a certificate of occupancy.

**SECTION 5.** That the property owner shall be responsible to maintain all required landscaping areas. This specifically includes replacement of damaged, removed, or dead plants, proper irrigation, fertilizing, pruning, mowing, and weeding as necessary to insure high quality landscaping appearances at all times.

**SECTION 6.** That all existing and proposed mechanical equipment (ground or roof mounted) shall be screened from public view. Roof-mounted mechanical equipment shall be screened by design, with screening measures clearly indicated in the building permit application package.

**SECTION 7.** That the building permit application shall include a soils report. Paving sections shall be designed based on the results of the soils report, to City Standards and acceptance of the director of city services.

**SECTION 8.** That the building permit application shall include a lighting plan for all exterior lighting. Exterior lighting shall be positioned and shielded as necessary to prevent light or glare from impinging on adjacent properties and roads.

**SECTION 9.** That a site grading and drainage plan shall be submitted to the city for review and approval.

**SECTION 10.** That city and State recycling requirements shall be met.

**SECTION 11.** That a city-standard, split-faced masonry trash and recycling enclosure with screened gates shall be constructed with adequate capacity to accommodate the waste generated by any and all businesses located on the site, to the satisfaction of the director of city services.

**SECTION 12.** That the following CEQA mitigation measures shall be adhered to and monitored by the City building inspector:

- Aesthetics: The project shall incorporate standard light shielding measures for street light fixtures to mitigate any potential adverse glare impacts.

- Air Quality: The project shall be subject to all applicable mandatory air pollution control measures of the San Joaquin Valley Unified Air Pollution Control District in effect at time of development, including, but not limited to: Regulation VIII (Fugitive PM10 Prohibitions), Regulation VIII (Rules 8011-8081), Rule 4102 (Nuisance), 4103 (Open Burning), Rule 4601 (Architectural Coatings), Rule 4641 (Cutback, Slow Cure, and Emulsified Asphalt, Paving and Maintenance Operations), Rule 4901 (Wood Burning Fire Places and Wood Burning Heaters), and Rule 9510 (Indirect Source Review). The developer would specifically demonstrate compliance with San Joaquin Valley Air Pollution Control District Rule 9510 (Indirect Source Review), including payment of all applicable fees, prior to the issuance of the first building permit.

- Cultural Resources: Pursuant to CEQA Guidelines 15064.5 (f), provisions for historical or unique archaeological resources accidentally discovered during construction shall be instituted. Therefore, in the event that any prehistoric or historic subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be halted and a qualified archaeologist or paleontologist shall be contacted to assess the significance of the find. If any find is determined to be significant, project proponents and the qualified archaeologist and/or paleontologist shall meet to determine the appropriate avoidance measures or other appropriate mitigation. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and a report prepared by the qualified archaeologist according to current professional standards. If the discovery includes human remains, CEQA Guidelines 15064.5 (e)(1) shall be followed.

- Noise: High noise levels resulting from construction activities shall be limited to the hours of 6:00 a.m. to 10:00 p.m., including weekends and holidays.

**SECTION 13.** That project development shall be coordinated with Caltrans, including but not limited to the following Caltrans requirements and recommendations:

- No new direct access from the proposed development to SR 65 shall be allowed.
- If a driveway is to be shared by two or more property owners, an access easement (or an agreement acceptable to the State) shall be executed between the parties and submitted to the Encroachment Permit office before a permit is issued for any work in the State right of way.

- Any existing or proposed driveways accessing State right-of-way shall meet current State standards.

- A sidewalk and landscape maintenance agreement shall be required.
- An encroachment permit shall be obtained from Caltrans for all proposed activities for placement of encroachments within, under or over the State highway rights-of-way. Activity and work planned in the State right-of-way shall be performed to State standards and specifications, at no cost to the State. Engineering plans, calculations, specifications, and reports (documents) shall be stamped and signed by a licensed Engineer or Architect. Engineering documents for encroachment permit activity and work in the State right-of-way may be submitted using English Units. The Permit Department and the Environmental Planning Branch shall review and approve the activity and work in the State right-of-way before an encroachment permit is issued. Encroachment permits shall be issued in accordance with Streets and Highway Codes, Section 671.5, "Time Limitations." Encroachment permits do not run with the land. A change of ownership requires a new permit application. Only the legal property owner or his/her authorized agent shall pursue obtaining an encroachment permit.

- Advertising signs within the immediate area outside the State right-of-way shall be cleared through the Caltrans Division of Traffic Operations, Office of Outdoor Advertising. The project proponent must construct and maintain the advertising signs without access to the State Routes.

- Any proposed landscaping plans shall meet current standards as determined by the District Landscape Architect. Proposed landscaping shall be low growing, less than two feet in height, due to sight distance concerns. All features of landscaping shall be evaluated for type, location and site

visibility conflicts during the encroachment review process. All permits for landscaping in conventional highway right-of-way shall be accompanied by a "District" approved maintenance agreement obligating a local agency or the permittee to maintaining the landscaping. Said maintenance agreement shall accompany and be approved prior to issuance of the landscape permit. Proposed landscape projects in access control rights-of-way require an exception process, and approval is subject to the Headquarters Departmental approval process.

- Dust control measures shall be implemented on the site in a manner to prevent dust from entering the State right-of-way.
- No water from the proposed project shall flow into the State right-of-way without approval from the District Hydraulic Engineer.

**SECTION 14.** That all other city codes and ordinances shall apply.

**PASSED, APPROVED AND ADOPTED** by the City Council of the City of Lindsay this 14th day of July, 2015.

ATTEST:

CITY COUNCIL OF THE CITY OF LINDSAY

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Carmela Wilson, City Clerk

---

Ramona Villarreal-Padilla, Mayor



# Initial Study/Proposed Negative Declaration

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New Subway Restaurant (site plan review [SPR]  
15-03)

William Zigler  
3/4/2015

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## 1.0 PROJECT INFORMATION

|                                    |   |
|------------------------------------|---|
| Project Title:                     | New Subway Restaurant (SPR 15-03)   |
| Lead Agency:                       | City of Lindsay, 251 E. Honolulu St, Lindsay, CA 93247  |
| Contact Person:                    | William Zigler (559) 562-7102 ext. 8032   |
| Location:                          | City of Lindsay   |
| Applicant:                         | Maninder S. Sandhu<br>1163 E. Pinehurst Ave.<br>Fresno, CA 93730  |
| General Plan Land Use Designation: | Highway commercial.   |
| Zoning:                            | Highway commercial (CH).  |
| Description of Project:            | See Project Description in Section 3 of this Initial Study.   |
| On-Site Land Uses:                 | Parking and green space.  |
| Surrounding Land Uses:             | Highway commercial land use to the north and east, highway commercial land use to the west, separated by Highway 65, and central commercial land use to the south, separated by Hermosa Street. |
| Interested Agencies:               | Caltrans and San Joaquin Valley Air Pollution Control District.   |

## 2.0 INTRODUCTION

### 2.1 Initial Study/Mitigated Negative Declaration

The purpose of this Initial Study/Mitigated Negative Declaration (IS/MND) is to identify the potential environmental impacts associated with the proposed new Subway restaurant project (SPR 15-03) and to describe measures that will avoid or mitigate impacts to a less than significant level. The IS/MND includes information to substantiate the conclusion made regarding the potential of the proposed project to result in significant environmental impacts and provides the basis for input from public agencies, organizations, and interested members of the public. Pursuant to Section 15367 of the California Environmental Quality Act (CEQA) Guidelines, the City of Lindsay is the Lead Agency for the proposed project, and as such, has primary responsibility for approval or denial of the proposed project.

The IS/MND has been prepared in accordance with CEQA Statutes and Guidelines, including Section 15070-15075 of the State CEQA Guidelines. Pursuant to Public Resources Code (PRC) Section 21157.1 and State CEQA Guidelines Section 15177, this project has been evaluated with respect to each item on the State CEQA Guidelines Appendix G environmental checklist to determine whether this project may cause a significant impact. The IS/MND has concluded that the proposed project would not result in any adverse effects which fall within the "Mandatory Findings of Significance" contained in Section 15065 of the State CEQA Guidelines.

### 2.2 Public and Agency Review

This Initial Study will be circulated for public and agency review from March 25, 2015 to April 13, 2015. Copies of this document are available for review at the following locations:

City of Lindsay Planning and Economic Development office:  
251 E. Honolulu Street  
Lindsay, California 93247  
(559) 562-7102 ext. 8032

The document is also available on the City of Lindsay website at:  
<http://www.lindsay.ca.us/documents/Planning/EnvironmentalDocs.htm>

### 2.3 Project Approvals

As a public agency principally responsible for approving or carrying out the proposed project, the City of Lindsay is the Lead Agency under CEQA and is responsible for adopting the environmental document and approving the proposed project. The discretionary approval would be required from the City Council. Approval of the Pedestrian Pathways, Roosevelt Elementary School project is anticipated to occur at the same time as the CEQA document adoption.

### 2.4 Organization of the Initial Study

This Initial Study is organized into the following sections:

**Section 1 – Project Information:** provides summary background information about the proposed project, including project location, lead agency, and contact information.

**Section 2 – Introduction:** summarizes the scope of the document, the project's review and approval processes, and the document's organization.

**Section 3 – Project Description:** presents a description of the proposed project, including the need for the project, the project’s objectives, and the elements included in the project.

**Section 4 – Environmental Factors Potentially Affected:** addresses whether this Initial Study identifies any environmental factors that involve a significant or potentially significant impact that cannot be reduced to a less than significant level.

**Section 5 – Determination:** indicates whether impacts associated with the proposed project would be significant and what, if any, additional environmental documentation is required.

**Section 6 – Evaluation of Environmental Impacts:** contains the Environmental Checklist form for each resource area. The checklist is used to assist in evaluating the potential environmental impacts of the proposed project. This section also presents a background summary for each resource area, and an explanation of all checklist answers.

**Section 7 – Mandatory Findings of Significance:** indicates whether implementation of the proposed project would result in significant environmental impacts.

**Section 8 – Mitigation Measures:** lists all mitigation measures proposed to be included as part of the proposed project.

**Section 9 – References:** lists references used in the preparation of this document.

### **3.0 PROJECT DESCRIPTION**

#### **3.1 Project Summary**

The New Subway Restaurant project (SPR 15-03) is a request by Maninder S. Sandhu to construct a new Subway restaurant at the NE corner of Hwy 65 and Hermosa, adjacent to the existing Country Waffle restaurant on the southernmost portion of a .95 acre site (APN: 199-280-002). Specifically, the project would be located on the south parking area and adjacent lawn. The project would share site access and parking with Country Waffle and the Super 8 Motel and would include a drive-through pickup window at the Subway. An overview, aerial photo, and site drawings with elevations are provided, as identified below.

**Figure 1** Overview: shows the project location within the city and zoning.

**Figure 2** Site Aerial: shows proposed location and surrounding development.

**Figure 3** Site Plan: shown proposed project on site.

**Figure 4** Site Interior: shows proposed interior layout.

**Figure 5** Building elevations: shows proposed exterior elevations.

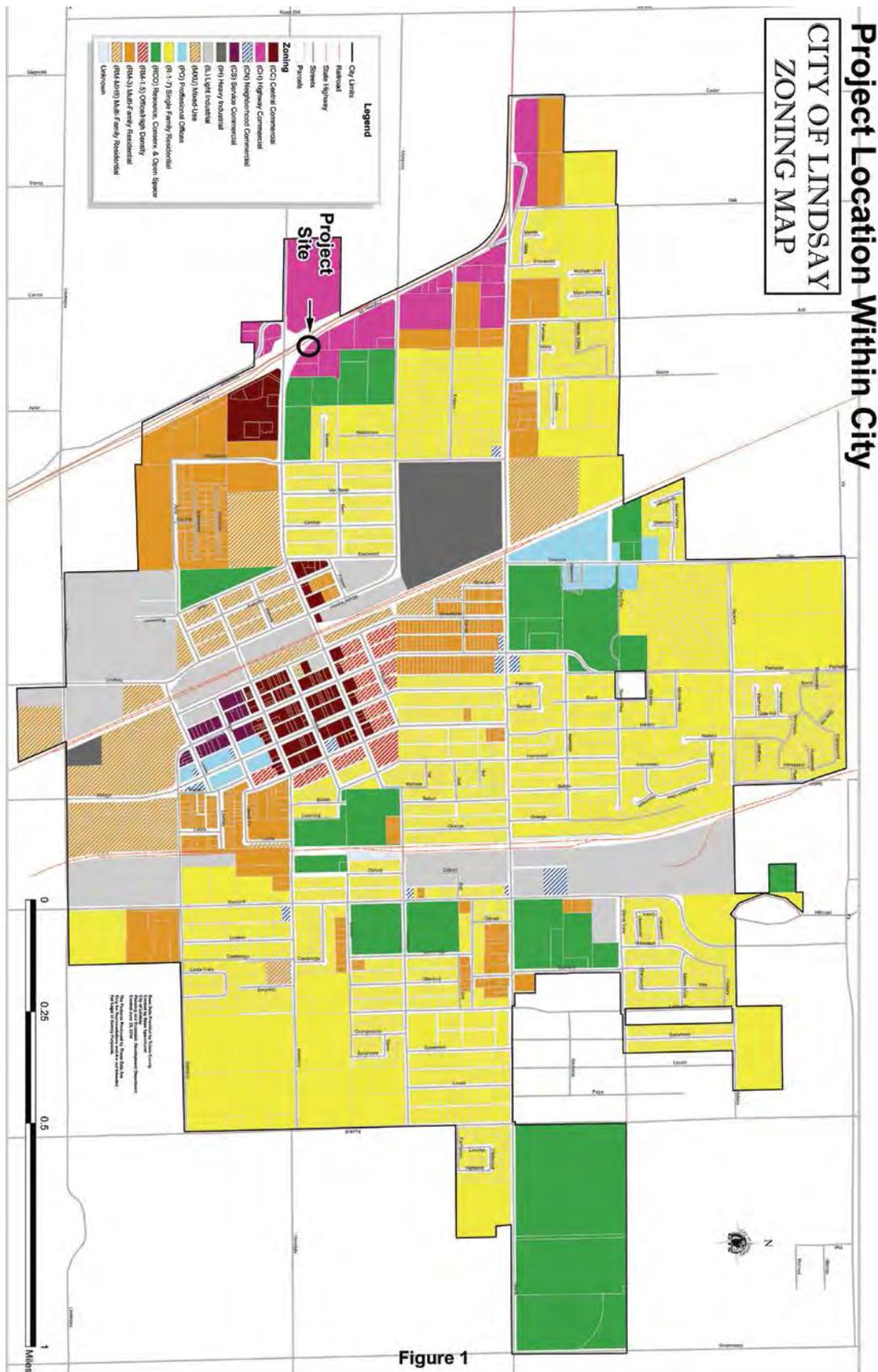
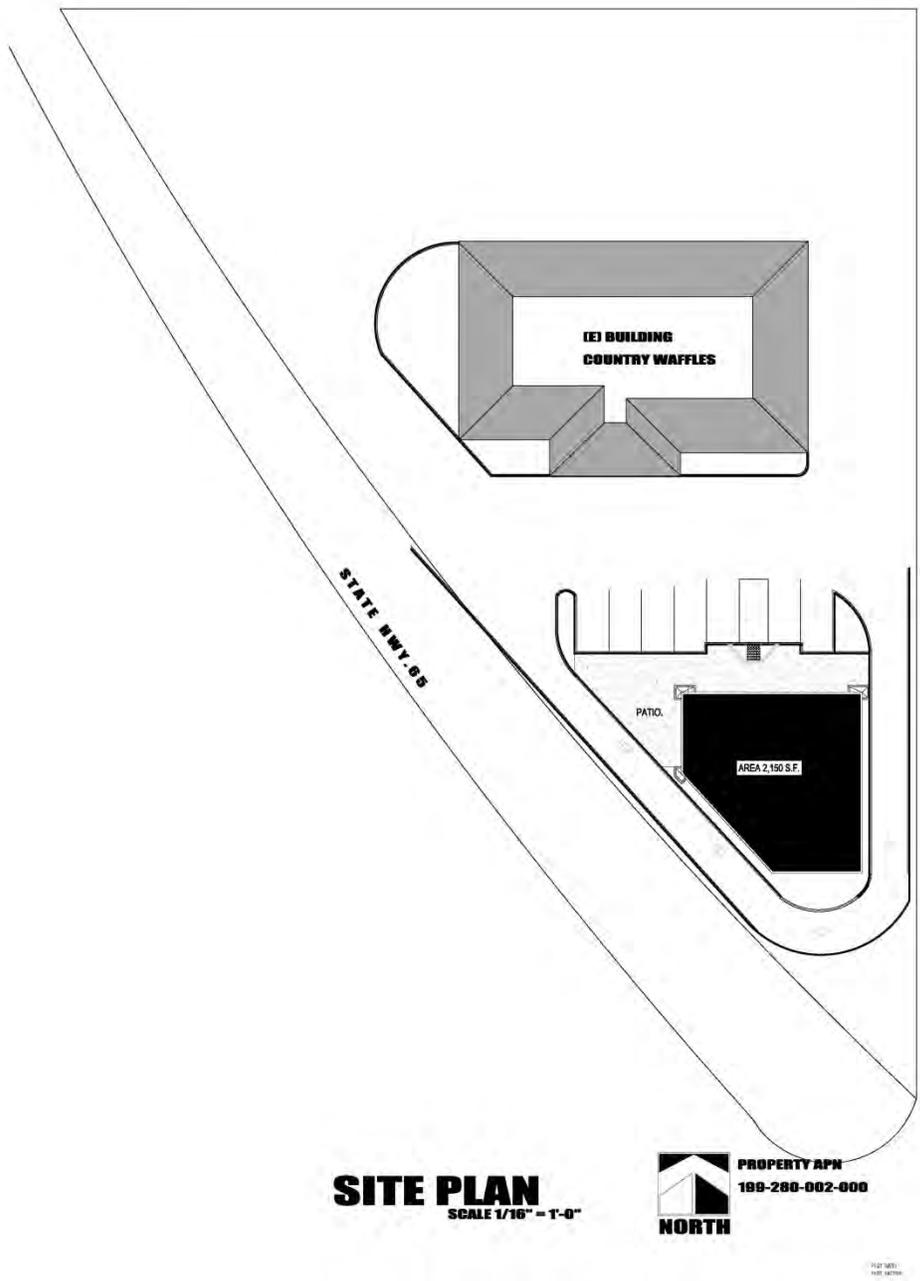


Figure 1 Overview: Project location within City of Lindsay



Figure 2 Site Aerial Photo



3711 W. LOCUST AVE  
FRESNO, CA 93711  
PH: 559.330.2219  
melde@garch.com

ARCHITECTURE  
PLANNING  
INTERIORS



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MELISSA ARCHIBALD ARCHITECTS  
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OFFICE OF THE ARCHITECT.  
NO PART OF THIS DRAWING  
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OR BY ANY INFORMATION  
SYSTEM, WITHOUT THE WRITTEN  
CONSENT OF MELISSA ARCHIBALD  
ARCHITECTS. ALL RIGHTS RESERVED.

REV  
A

BUILDING ADDITION  
380 CALIFORNIA 65  
LINDSAY CA93247

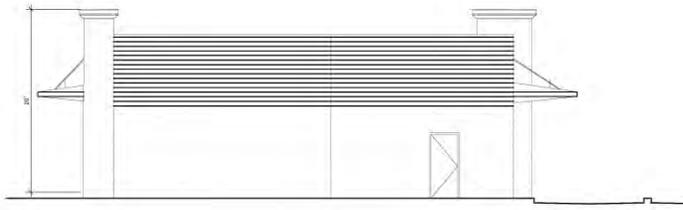
DATE: 12-22-2014

A0.0

AM CHECK # 15-1000

Figure 3 Site Plan

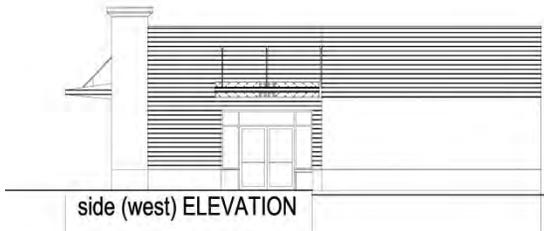




REAR (NORTH) ELEVATION

**KEYED NOTES**

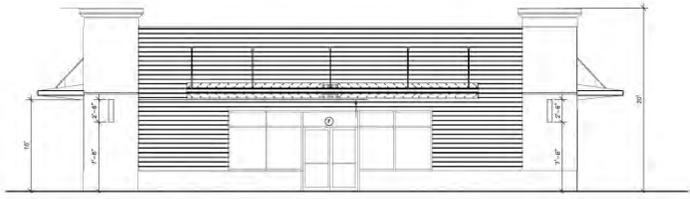
- 1. AS SHOWN, ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE BUILDING CODES AND ALL APPLICABLE REGULATIONS.
- 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AGENCIES.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AGENCIES.
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- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AGENCIES.
- 9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AGENCIES.
- 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AGENCIES.



side (west) ELEVATION



side (east) ELEVATION



**FRONT ELEVATION** SCALE 1/4" = 1'-0"

BUILDING ADDITION  
 380 CALIFORNIA 65  
 LINDSAY CA93247  
 DATE: 12-22-2014  
 A2.0

Figure 5 Building Elevations.

## 3.2

### Project Background and Objectives

**Background:** The project site is part of the Country Waffle property, created in 1987 by Parcel Map 3487, which split a 3.14 acre lot into two parcels to accommodate a motel (now Super 8) to the north and a restaurant (now Country Waffle) to the south. Parcel 1 (Super 8) is 2.19 acres and Parcel 2 (Country Waffle) is .95 acres in area. The map included a non-exclusive easement for parcels 1 and 2 for driving, parking, and walking in areas designated as the parking lot and adjoining driveways and sidewalks as well as a reciprocal drainage easement. Parcel Map 3487 additionally required a thirty foot wide setback line located along the easterly line of parcels 1 and 2.

Surrounding land uses for the project site include:

- North: Super 8 Motel (highway commercial use).
- South: Hermosa Street and to the south of Hermosa Street, the Olivewood Plaza.
- East: Burger King (highway commercial use) and vacant land.
- West: Highway 65 and to the west of Hwy 65, vacant land.

**Objectives:** Project objectives include creating a drive-through opportunity for Subway restaurant customers in a new location, developing an underutilized portion of commercially zoned property, and reducing landscape irrigation water usage for private, commercially owned property.

## 3.3

### Project Site and Surrounding Uses

The New Subway restaurant project would occur within the southernmost portion of a substantially developed .95 acre parcel. The project site is comprised of a portion of a parking lot and adjacent lawn area. The project site is bordered by commercial uses (highway commercial and central commercial) and transportation corridors.

Surrounding land uses for the project site include:

- North: Super 8 Motel.
- South: Hermosa Street and Olivewood Plaza.
- East: Burger King.
- West: Highway 65 and vacant land.

## 3.4 Construction Schedule and Activities

The proposed project includes the construction of the restaurant, a drive-through, patio and sidewalk areas, refurbished parking, including van-accessible handicapped parking, lighting, signage, and landscaping. The project would be constructed entirely on privately owned property. There are no right-of-way or property acquisitions included as part of this proposed project. Construction of the proposed project is anticipated to begin during the summer of 2015.

**4.0 ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:**

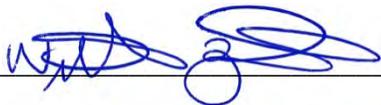
The environmental factors checked below would be potentially affected by this project involving at least one impact that is a “Less than Significant Impact with Mitigation” as indicated by the checklist on the following pages.

|   |                               |   |                                    |
|---|-------------------------------|---|------------------------------------|
| X | Aesthetics                    |   | Agricultural Resources             |
| X | Air Quality                   |   | Biological Resources               |
| X | Cultural Resources            |   | Greenhouse Gases                   |
|   | Geology and Soils             |   | Hazards                            |
|   | Hydrology and Water Quality   |   | Land Use and Planning              |
|   | Mineral Resources             | X | Noise                              |
|   | Population and Housing        |   | Public Services                    |
|   | Recreation                    |   | Transportation/Circulation         |
|   | Utilities and Service Systems |   | Mandatory Findings of Significance |

## 5.0 DETERMINATION

On the basis of this initial evaluation:

|   |  |
|---|--|
|   | I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.  |
| X | I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the proposed proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.  |
|   | I find that the proposed project MAY have a significant effect on the environment and an ENVIRONMENTAL IMPACT REPORT is required.  |
|   | I find that the proposed project MAY have a “potentially significant impact” or “potentially significant unless mitigated” impact on the environment, but at least one effect (1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) has been addressed by mitigation measure based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT will be prepared. |

Signature:  \_\_\_\_\_

Date: 3/4/2015

William Zigler, Director of Planning and Economic Development  
City of Lindsay

## 6.0 EVALUATION OF ENVIRONMENTAL IMPACTS

This section includes an evaluation of impacts based on the *State CEQA Guidelines* Appendix G Environmental Checklist. Each checklist item is explained in the discussion following the checklist and, if necessary, mitigation measures are provided to reduce impacts to a less than significant level. In accordance with CEQA, all answers take into account the whole of the action, including on- and off-site effects, cumulative and project level; direct and indirect effects, and effects from both construction and operation of any new development.

Each checklist criterion is marked to identify whether there is an environmental impact.

- A “No Impact” response indicates that there is no impact.
- A “Less Than Significant Impact” response means that while there is some impact, the impact is below the threshold of significance defined by the City.
- A “Less Than Significant Impact with Mitigation” response indicates that a new impact has been identified in the course of this analysis and mitigation measures have been provided in this Initial Study to reduce a potentially significant impact to a less than significant level.

If a significant impact is identified that could not be reduced to a less than significant level, the box “Potential Significant Impact” would be checked. According to CEQA, if such an impact were identified, an Initial Study would not be sufficient to approve the project, and an Environmental Impact Report (EIR) would be necessary. No such impacts have been identified in the course of preparing this Initial Study.

## 6.1 Aesthetics

|   | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact        | No Impact                           |
|---|--------------------------------|---------------------------------------|-------------------------------------|-------------------------------------|
| <b>AESTHETICS:</b> Would the project:   |                                |                                       |                                     |                                     |
| a) Have a substantial adverse effect on a scenic vista  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| c) Substantially degrade the existing visual character or quality of the site and its surroundings?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?                                   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |

### **Discussion:**

a – b). No impact. There are no designated State Scenic Highways located within or adjacent to the project area. The project site is located near the western extent of the city’s urban development and is generally bordered by highways, commercial uses, and vacant land. Views of foothills are currently available from the project site only to the northeast. All other views are blocked by existing structures. The project involves the installation of the restaurant, a drive-through, patio and sidewalk areas, refurbished parking, including van-accessible handicapped parking, lighting, signage, and landscaping as discussed above. The site does not have an identified scenic vista, nor is it part of a scenic vista. There are no other identified significant scenic resources on the project site. Since the project area is substantially developed, the visual character of the site and its surroundings will not be degraded.

c – d). Less than significant impact. The project area is substantially developed; however, grass and several trees will be removed to facilitate construction. As with any urban development the project will require installation of standard street lighting. The project will incorporate standard light shielding measures for street light fixtures to mitigate any potential adverse glare impacts.

## 6.2 Agricultural Resources

**AGRICULTURE AND FOREST RESOURCES:** In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment Project; and the forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

|  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|--|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| d) Result in the loss of forest land or conversion of forest land to non-forest use?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

### **Discussion:**

No impact. The project will be completed on substantially developed commercial property. The project does not conflict with existing zoning for agricultural use and does not involve other changes in the existing environment related to agricultural or forest uses that have not already been addressed in the existing General Plan.

### 6.3 Air Quality

| AIR QUALITY: Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact        | No Impact                |
|---|--------------------------------|---------------------------------------|-------------------------------------|--------------------------|
| a) Conflict with or obstruct implementation of the applicable air quality plan?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>   | <input type="checkbox"/>            | <input type="checkbox"/> |
| c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| d) Expose sensitive receptors to substantial pollutant concentrations?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| e) Create objectionable odors affecting a substantial number of people?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

**Discussion:**

a). Less Than Significant Impact. The proposed project is located within the boundaries of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAPCD is responsible for bringing air quality in the City into compliance with federal and state air quality standards. The proposed project does not include land use changes that would conflict with the long-range air quality projects of the San Joaquin Valley Air Pollution Control district. The current land use designation for the proposed project is Highway Commercial, as outlined in the City’s General Plan and the project would be consistent with the land use designation for the site, as adopted in the City of Lindsay General Plan. Since the project would not result in a change of land use, there would not be an increase in vehicle miles traveled unaccounted for in regional emissions inventories. Therefore, the project would not conflict with or obstruct implementation of any SJVAPCD plans or guidelines and impacts would be less than significant.

b). Less Than Significant Impact with Mitigation. Construction of the proposed project involves grading, excavation, and use of construction equipment. Project construction would result in short-term air pollutant emissions from use of construction equipment, earth-moving activities (grading), construction workers’ commutes, materials deliveries and short-distance earth and debris hauling.

To aid in evaluating potentially significant construction and/or operational impacts of a project, SJVAPCD has prepared an advisory document, the Guide for Assessing and Mitigating Air Quality Impacts (GAMAQI), which contains standard procedures for addressing air quality in CEQA documents (SJVAPCD, 2002) The guide was adopted in 1998 and revised in 2002.

GAMAQI presents a three-tiered approach to air quality analysis. The Small Project Analysis Level (SPAL) is first used to screen the project for potentially significant impacts. A project that meets the screening criteria at this level requires no further analysis and air quality impacts of the project may be deemed less than significant. If a project does not meet all the criteria at this screening level, additional screening is recommended at the Cursory Analysis Level and, if warranted, the Full Analysis Level.

Table 1 below (from GAMAQI 5-3(b), which SJVAPCD recommends using as part of the initial screening process, shows the maximum trips per day to be considered a SPAL project. According to the ITE Trip Generation Report (7<sup>th</sup> Edition), the operation of a 2,150 square foot fast food restaurant with drive-through window would result in approximately 1,174 daily trips. Therefore, the project meets the SPAL criterion for project type and is excluded from quantifying criteria pollutant emissions for CEQA purposes.

**Table 1**  
**Small Project Analysis Level (SPAL) by vehicle trips**

| <b>Land Use Category</b> | <b>Project Size</b> |
|--------------------------|---------------------|
| Residential Housing      | 1,453 trips/day     |
| Commercial               | 1,673 trips/day     |
| Office                   | 1,628 trips/day     |
| Institutional            | 1,707 trips/day     |
| Industrial               | 1,506 trips/day     |

Source: SJAPCD-GAMAQI, 2002

SJVAPCD Regulation VIII mandates requirements, as seen in Table 2, for any type of ground moving activity and would be adhered to during the construction. In addition to Regulation VIII, the project shall be subject to all applicable mandatory air pollution control measures of the San Joaquin Valley Unified Air Pollution Control District in effect at time of development, including, but not limited to: Regulation VIII (Fugitive PM10 Prohibitions), Rule 4102 (Nuisance), 4103 (Open Burning), Rule 4601 (Architectural Coatings), Rule 4641 (Cutback, Slow Cure, and Emulsified Asphalt, Paving and Maintenance Operations) Rule 4901 (Wood Burning Fire Places and Wood Burning Heaters), and Rule 9510 (Indirect Source Review). The developer shall specifically demonstrate compliance with San Joaquin Valley Air Pollution Control District Rule 9510 (Indirect Source Review), including payment of all applicable fees, prior to the issuance of the first building permit. This measure will be monitored by the City of Lindsay through the plan check process and construction. During construction, air quality impacts would be less than SJVAPCD thresholds for non-attainment pollutants and operation of the project would not result in impacts to air quality standards for criteria pollutants. As such, any impacts would be less than significant.

**Table 2**  
**SJVAPCD Regulation VIII Measures**

|   |
|---|
| <p>The following controls are required to be implemented at all construction sites in the San Joaquin Valley Air Basin</p> <ul style="list-style-type: none"> <li>• All disturbed areas, including storage piles, which are not being actively utilized for construction purposes, shall be effectively stabilized of dust emissions using water, chemical stabilizer/suppressant, covered with a tarp or other suitable cover or vegetative ground cover.</li> <li>• All on-site unpaved roads and off-site unpaved access roads shall be effectively stabilized of dust emissions using water or chemical stabilizer/suppressant.</li> <li>• All land clearing, grubbing, scraping, excavation, land leveling, grading, cut &amp; fill, and demolition activities shall be effectively controlled of fugitive dust emissions utilizing application of water or by presoaking.</li> <li>• With the demolition of buildings up to six stories in height, all exterior surfaces of the building shall be wetted during demolition.</li> <li>• When materials are transported off-site, all materials shall be covered, or effectively wetted to limit visible dust emissions, and at least six inches of freeboard space from the top of the container shall be maintained.</li> <li>• All operations shall limit or expeditiously remove the accumulation of mud or dirt from adjacent public streets at the end of each workday. (The use of dry rotary brushes is expressly prohibited except where preceded or accompanied by sufficient wetting to limit the visible dust emissions). (Use of blower devices is expressly forbidden).</li> <li>• Following the addition of materials to, or the removal of materials from, the surface of outdoor storage piles, said piles shall be effectively stabilized of fugitive dust emissions utilizing sufficient water or chemical stabilizer/suppressant.</li> <li>• Within urban areas, trackout shall be immediately removed when it extends 50 or more feet from the site and at the end of each workday.</li> <li>• Any site with 150 or more vehicle trips per day shall prevent carryout and trackout.</li> </ul> |
|---|

c). Less Than Significant Impact. The SJVAPCD accounts for cumulative impacts to air quality in its “Guide for Assessing and Mitigating Air Quality Impacts” Technical Document Information for Preparing Air Quality Sections in EIRs” and its “Guide for Assessing and Mitigating Air Quality Impacts”. The SJVAPCD considered basin-wide cumulative impacts to air quality when developing its significance thresholds (SJVAPCD, 2002b). The number of vehicle trips per year required to operate the proposed project would be substantially less than expected from a project requiring a quantitative analysis by the SJVAPCD. The operation of the proposed project would result in impacts to air quality far below those considered to be significant. As a result, the cumulative impacts to air quality from construction/operation of the proposed project are considered to be less than significant.

d). Less Than Significant Impact. The site is surrounded on all sides by urban uses (State Highway 65 and highway commercial). The project does not include any project components identified by the California Air Resources Board that could potentially impact any sensitive receptors. These include heavily traveled roads, distribution centers, fueling stations and dry cleaning operations. The proposed project would not expose sensitive receptors to substantial pollutant concentrations and therefore there will be less than significant impacts.

e). Less Than Significant Impact. The project will create temporary typical construction odors as the project develops. The proposed project will not introduce a conflicting land use (surrounding land includes State Highway 65 and highway commercial) to the area and will does not have any component that would typically emit odors. The project would not create objectionable odors affecting a substantial number of people and therefore there will be less than significant impacts.

## 6.4 Biological Resources

| <b>BIOLOGICAL RESOURCES:</b> Would the project:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|--|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

**Discussion:**

No impact. The project site is located near the western extent of the city’s urban development and is surrounded by urban uses. The lawn area and trees are routinely sprayed for pest/weed control and the area is regularly mowed. The project site has no identified biological resources that would be impacted by the parameters of this project. The project would not conflict with any local policies or ordinances protecting biological resources, since there are no such policies or ordinances. The project would not conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan, since none apply to the project area.

**6.5 Cultural Resources**

| CULTURAL RESOURCES: Would the project:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                |
|---|--------------------------------|---------------------------------------|------------------------------|--------------------------|
| a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?    | <input type="checkbox"/>       | <input checked="" type="checkbox"/>   | <input type="checkbox"/>     | <input type="checkbox"/> |
| b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5? | <input type="checkbox"/>       | <input checked="" type="checkbox"/>   | <input type="checkbox"/>     | <input type="checkbox"/> |
| c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?       | <input type="checkbox"/>       | <input checked="" type="checkbox"/>   | <input type="checkbox"/>     | <input type="checkbox"/> |
| d) Disturb any human remains, including those interred outside of formal cemeteries?                          | <input type="checkbox"/>       | <input checked="" type="checkbox"/>   | <input type="checkbox"/>     | <input type="checkbox"/> |

**Discussion:**

a – d). Less Than Significant with Mitigation. There are no known historical, archaeological or paleontological resources located within the project area; however, it is impossible to know if undiscovered underground historical resources are present. Implementation of the mitigation measure below will ensure that impacts to this checklist item will be less than significant with mitigation incorporation.

Pursuant to CEQA Guidelines 15064.5 (f), provisions for historical or unique archaeological resources accidentally discovered during construction should be instituted. Therefore, in the event that any prehistoric or historic subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be halted and a qualified archaeologist or paleontologist shall be contacted to assess the significance of the find. If any find is determined to be significant, project proponents and the qualified archaeologist and/or paleontologist would meet to determine the appropriate avoidance measures or other appropriate mitigation. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and a report prepared by the qualified archaeologist according to current professional standards. If the discovery includes human remains, CEQA Guidelines 15064.5 (e)(1) shall be followed.

## 6.6 Geology and Soils

| <b>GEOLOGY AND SOILS:</b> Would the project:   | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|--|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| ii) Strong seismic ground shaking?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| iii) Seismic-related ground failure, including liquefaction?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| iv) Landslides?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Result in substantial soil erosion or the loss of topsoil?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

### **Discussion:**

No impact. The project will consist of constructing a single-story restaurant and installing ground-level flatwork (curbing, gutters, patio and sidewalk), lighting, and supporting infrastructure. The project will not expose people or structures to potential substantial adverse effects involving the rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault; strong seismic ground shaking; seismic-related ground failure, including liquefaction; or landslides.

The site is level and surrounded by other similarly situated properties. The project will not result in soil erosion or the substantial loss of topsoil. The site has no significant topographical or geologic features which would contribute to adverse geologic or soil impacts associated with this project. The project could involve minor excavation and grading and may include the use of fill; however, these actions are not anticipated to be substantial or to have the potential for a significant impact on site geology or soils.

## 6.7 Greenhouse Gas Emissions

| <b>GREENHOUSE GAS EMISSIONS:</b> Would the project:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact        | No Impact                           |
|--|--------------------------------|---------------------------------------|-------------------------------------|-------------------------------------|
| a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?      | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

### **Discussion:**

Greenhouse gas (GHG) emissions refer to a group of emissions that are believed to affect global climate conditions. These gases trap heat in the atmosphere and the major concern is that increases in GHG emissions are causing global climate change. Global climate change is a change in the average weather on earth that can be measured by wind patterns, storms, precipitation, and temperature. Although there is disagreement as to the speed of global warming and the extent of the impacts attributable to human activities, most agree that there is a direct link between increased emission of GHGs and long-term global temperature. What GHGs have in common is that they allow sunlight to enter the atmosphere, but trap a portion of the outward-bound infrared radiation and warm up the air. The process is similar to the effect greenhouses have in raising the internal temperature, hence the name greenhouse gases. Both natural processes and human activities emit GHGs. The accumulation of greenhouse gases in the atmosphere regulates the earth's temperature; however, emissions from human activities such as electricity generation and motor vehicle operations have elevated the concentration of GHGs in the atmosphere. This accumulation of GHGs has contributed to an increase in the temperature of the earth's atmosphere and contributed to global climate change.

The principal GHGs are carbon dioxide (CO<sub>2</sub>), methane (CH<sub>4</sub>), nitrous oxide (N<sub>2</sub>O), sulfur hexafluoride (SF<sub>6</sub>), perfluorocarbons (PFCs), hydrofluorocarbons (HFCs), and water vapor (H<sub>2</sub>O). CO<sub>2</sub> is the reference gas for climate change because it is the predominant greenhouse gas emitted. To account for the varying warming potential of different GHGs, GHG emissions are often quantified and reported as CO<sub>2</sub> equivalents (CO<sub>2</sub>e).

In 2005, in recognition of California's vulnerability to the effects of climate change, Governor Schwarzenegger established Executive Order S-3-05, which sets forth a series of target dates by which statewide emission of GHG would be progressively reduced, as follows:

- By 2010, reduce greenhouse gas emission to 2000 levels;
- By 2020, reduce greenhouse gas emission to 1990 levels; and
- By 2050, reduce greenhouse gas emissions to 80 percent below 1990 levels.

In response to Executive Order S-3-05, the Secretary of Cal/EPA created the Climate Action Team (CAT), which, in March 2006, published the Climate Action Team Report to Governor Schwarzenegger and the Legislature (2006 CAT Report). The 2006 CAT Report identified a recommended list of strategies that the state could pursue to reduce climate change greenhouse gas emissions. These are strategies that could be implemented by various state agencies to ensure that the Governor's targets are met and can be met with existing authority of the state agencies.

In 2006, California passed the California Global Warming Solutions Act of 2006 (Assembly Bill No. 32; California Health and Safety Code Division 25.5, Section 38500, et seq., or AB 32), which requires the California Air Resources Board (ARB) to design and implement emission limits, regulations, and other measures, such that feasible and cost-effective statewide GHG emissions are reduced to 1990 levels by 2020.

As a central requirement of AB 32, the ARB was assigned the task of developing a Climate Change Scoping Plan that outlines the state's strategy to achieve the 2020 GHG emissions limits. This Scoping Plan, which was developed by the ARB in coordination with the CAT, includes a comprehensive set of actions designed to reduce overall GHG emissions in California, improve the environment, reduce the state's dependence on oil, diversify the state's energy sources, save energy, create new jobs, and enhance public health. An important component of the plan is a cap-and-trade program covering 85 percent of the state's emissions. Additional key recommendations of the Scoping Plan include strategies to enhance and expand proven cost-saving energy efficiency programs; implementation of California's clean cars standards; increases in the amount of clean and renewable energy used to power the state; and implementation of a low-carbon fuel standard that will make the fuels used in the state cleaner. Furthermore, the Scoping Plan also proposes full deployment of the California Solar Initiative, high-speed rail, water-related energy efficiency measures, and a range of regulations to reduce emission from trucks and from ships docked in California ports. The Climate Change Scoping Plan was approved by the ARB on December 22, 2008. According to the September 23, 2010 AB 32 Climate Change Scoping Plan Progress Report, 40 percent of the reductions identified in the Scoping Plan have been secured through ARB actions and California is on track to its 2020 goal.

Although not originally intended to reduce GHGs, California Code of Regulations (CCR) Title 24, Part 6: California's Energy Efficiency Standards for Residential and Nonresidential Buildings, was first adopted in 1978 in response to a legislative mandate to reduce California's energy consumption. Since then, Title 24 has been amended with recognition that energy-efficient buildings require less electricity and reduce fuel consumption, which in turn decreased GHG emissions. The current 2010 Title 24 standards were adopted to respond, amongst other reasons, to the requirements of AB 32. Specifically, new development projects within California after January 1, 2011 are subject to the mandatory planning and design, energy efficiency, water efficiency and conservation, material conservation and resources efficiency, and environmental quality measures of the California Green Building Standards (CALGreen) Code (California Code of Regulations, Title 24, Part 11).

a). – Less than significant impact.

Construction: Greenhouse gas emissions, generated during construction, would include activities such as site preparation, grading, the construction of the building, paving, etc. The SJVAPCD does not have a recommendation for assessing the significance to construction-related emissions. Construction activities occurring before 2020, the year when the State is required to reduce its GHG emissions to 1990 levels, are therefore considered less than significant.

Operation: The project will include long-term emissions over the lifetime of the project that include mobile operations, waste generated, water consumed, and energy consumed. The U.S. Environmental Protection Agency published a rule for the mandatory reporting of greenhouse gases (GHG) from sources that in general emit 25,000 metric tons or more of carbon dioxide equivalent (CO<sub>2</sub>e) per year. Project operational GHG emissions were calculated using CalEEMod based on .05 acres of development and a 2,150 square foot fast-food restraint with drive-through service. This project is estimated to produce 626.8 metric tons per year of CO<sub>2</sub>e, which is well below the 25,000 metric tons action threshold for greenhouse gas emissions. The CalEEMod output files can be seen in Attachment A.

b). – No impact. The project would not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases. The City of Lindsay has included a good faith effort in order to provide the public and decision-makers as much information as possible about the project. The City of Lindsay does remain firmly committed to implementing measures to help reduce the potential effects of the project.

## 6.8 Hazards and Hazardous Materials

| HAZARDS AND HAZARDOUS MATERIALS: Would the project:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|--|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?                                   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

### **Discussion:**

No impact. The project does not involve the use, disposal, or transport of hazardous materials. The project site is not known to be included in a hazardous materials site list. The project site is not located near a public use airport, and is not within areas of potential hazard created by existing public use airports. The project site is well-served by existing arterial and collector roads, and therefore would not impede emergency access required for emergency response and evacuation plans. Finally, the project site is not in an area identified for wildland fire hazards.

## 6.9 Hydrology and Water Quality

| <b>HYDROLOGY AND WATER QUALITY:</b> Would the project:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact        | No Impact                           |
|---|--------------------------------|---------------------------------------|-------------------------------------|-------------------------------------|
| a) Violate any water quality standards or waste discharge requirements?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| f) Otherwise substantially degrade water quality?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| j) Inundation by seiche, tsunami, or mudflow  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

### **Discussion:**

a). **Less Than Significant Impact:** The project itself will not violate any water quality standards of waste discharge requirements. The project will tie into an existing sewer line within the Caltrans right-of-way and discharge to the City's wastewater treatment plant. The City Services department will regularly monitor the waste water discharge to meet City requirements. This project will have minimal impacts on the water quality and waste discharge requirements and therefore there will be a less than significant impact.

b). **Less Than Significant Impact:** The City of Lindsay (and proposed Project site) is located in the Kaweah Sub-basin portion of the Tulare Lake Basin, an area significantly affected by overdraft. The Department of Water Resources (DWR) has estimated the groundwater by hydrologic region and for the Tulare Lake Basin; the total overdraft is estimated at 820,000 acre-feet per year, the greatest overdraft projected in the state, and 56 percent of the statewide total overdraft (Tulare County General Plan, 2012). The proposed project will connect to the City's water system, which is served by a mix of both ground and surface water.

Site landscaping is currently irrigated with city water. This irrigation requirement would end with the removal of the grass area. For comparison, the existing Lindsay Subway Restaurant uses approximately 9,912 gallons of water per month currently and it is expected that the water consumption within the new restaurant would likely decrease due to updated plumbing, fixtures, and policies.

The City has outlined a number of short and long term capital improvement projects to assist with providing its residents with adequate water supply. In addition, the project will be required to adhere to all City and State mandated water conservation measures and regulations. Therefore, the proposed project would not substantially deplete ground water supplies or interfere substantially with groundwater recharge. The project will result in less than significant impacts.

c – d). Less Than Significant Impact. The proposed project will alter the existing drainage pattern with the development of the project site; however, the project will be connected with the City’s existing stormwater drainage system. There are no rivers, streams, or other water courses that will be impacted with the development of this project, and therefore there will be less than significant impacts.

e). Less Than Significant Impact. The proposed project will tie into the City’s existing stormwater drainage system. Construction and grading activities would create a potential for surface water to carry sediment from onsite erosion into the storm water system and downstream waterways. However, implementation of adopted management practices and compliance with the provisions of the National Pollutant Discharge Elimination System (NPDES) permit will ensure that these impacts remain less than significant.

f). No Impact. The project is not a source which would otherwise create substantial degradation of water quality.

g – h). No Impact. The site is not within a 100-year flood hazard zone (FEMA Flood Insurance Rate Map, Panel 06107C1305E). There is no impact.

i – j) No Impact. Dam structure improvements to the Lake Kaweah dam raised the potential holding capacity at the lake by 21 feet. The dam at Lake Success has been undergoing a lengthy safety evaluation by the Army Corps of Engineers and the lake volume has been dramatically reduced during this period to ensure regional safety. The improvements at Lake Kaweah and cautionary measures taken at Lake Success should greatly reduce the potential of downstream flooding due to peak storm events. In the unlikely event of dam breach, floodwaters from either lake could potentially reach the Lindsay area. The project would not result in exposure of people or structures to a significant risk of loss, injury or death involving flooding resulting from a dam or levee breach, compared other areas in the Lindsay General Plan. The project site is not located in an area subject to seiche, tsunami, or mudflow hazards.

## 6.10 Land Use and Planning

| <b>LAND USE AND PLANNING:</b> Would the project:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|---|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Physically divide an established community?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

c) Conflict with any applicable habitat conservation plan or natural community conservation plan?

**Discussion:**

No impact. The project would not physically divide an established community, nor conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project adopted for the purpose of avoiding or mitigating environmental effects. There is no known habitat conservation plan or natural community conservation plan that includes the project site, so the project would therefore have no impact on such plans.

## 6.11 Mineral Resources

**MINERAL RESOURCES:** Would the project:

Potentially Significant Impact

Less Than Significant with Mitigation

Less Than Significant Impact

No Impact

a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

**Discussion:**

No impact. There are no known mineral resources or mineral resource recovery sites on or adjacent to the project site. The project will have no impact on mineral resources.

## 6.12 Noise

| NOISE: Would the project result in:   | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|---|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Exposure of persons to or generation of excessive ground-borne vibration or ground-borne noise levels?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?  | <input type="checkbox"/>       | <input checked="" type="checkbox"/>   | <input type="checkbox"/>     | <input type="checkbox"/>            |
| e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

### **Discussion:**

a – c) and e – f). No Impact. The project would not expose persons to generation of noise levels in excess of standards. The project would not expose persons to the generation of ground-borne vibrations or ground-borne noise. The project would not create a substantial permanent increase in ambient noise levels. The project is not within an airport land use plan, within two miles of an airport, nor is the project within the vicinity of a private airstrip.

d). Less Than Significant Impact with Mitigation. Construction activities associated with implementation of the proposed project could temporarily increase ambient noise levels. Typical construction equipment would include scrapers, backhoes, drilling rigs and miscellaneous equipment (i.e. pneumatic tools, generators and portable air compressors). Typical noise levels generated by this type of construction equipment at various distances from the noise source are scraper, dump truck, water, truck, backhoe, and generator. High noise levels resulting from construction activities shall be limited to the hours of 6:00 a.m. to 10:00 p.m, including weekends and holidays. Implementation of the mitigation would reduce impacts to less than significant.

## 6.13 Population and Housing

| <b>POPULATION AND HOUSING:</b> Would the project:   | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|---|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

### **Discussion:**

No impact. The project would not induce substantial population growth in an area, either directly or indirectly, nor would it displace substantial numbers of existing housing, nor would it displace substantial numbers of people.

## 6.14 Public Services

| <b>PUBLIC SERVICES:</b>   | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|---|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| Fire protection?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| Police protection?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| Schools?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| Parks?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| Other public facilities?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

### **Discussion:**

No impact. The project would not result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, nor create a need for new or physically altered governmental facilities. The project would not result in an increased need for fire protection, police protection, schools, or parks.

## 6.15 Recreation

| RECREATION:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|--|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?                        | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

### Discussion:

No impact. The project would not cause substantial or accelerated physical deterioration of park facilities. The project does not include recreational facilities.

## 6.16 Transportation/Traffic

| TRANSPORTATION/TRAFFIC: Would the project:  | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact        | No Impact                           |
|---|--------------------------------|---------------------------------------|-------------------------------------|-------------------------------------|
| a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that result in substantial safety risks?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| e) Result in inadequate emergency access?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| f) Conflict with adopted policies, plans or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

### Discussion:

a - b). Less Than Significant Impact. According to the ITE Trip Generation Report (7<sup>th</sup> Edition), the operation of a 2,150 square foot fast food restaurant with drive-through window would result in approximately 1,174 daily trips. Per a project review by Caltrans, stated in Letter, 6-TUL-65-29.53 +/-

2135-IGR/CEQA, FAST FOOD RESTAURANT, PRELIMINARY REVIEW, dated February 4, 2015 the project would not cause a significant increase in traffic and neither a traffic study nor mitigation are required. The project would not exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways.

c – f.) No impact. The project would not result in a change in air traffic patterns, including either an increase in traffic levels or a change in location. The project would not substantially increase hazards due to a design feature or incompatible uses. The project would not result in inadequate emergency access. The project would not result in inadequate parking capacity, having dedicated as well as shared parking capacity. The project would not conflict with adopted policies, plans, or programs supporting alternative transportation, but would rather support alternative transportation.

## 6.17 Utilities and Service Systems

| UTILITIES AND SERVICE SYSTEMS: Would the project:   | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact        | No Impact                |
|---|--------------------------------|---------------------------------------|-------------------------------------|--------------------------|
| a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?                            | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?                                     | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| g) Comply with federal, state, and local statutes and regulations related to solid waste?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

### Discussion:

a – g) Less Than Significant Impact. During construction, the project will utilize portable restroom facilities that will be provided by the construction contractor for the construction workers. The wastewater would be contained within the portable unit and disposed of at an approved site according to regulations. The project itself will not violate any water quality standards or waste discharge requirements. The project will tie into the existing sewer, water, and stormwater lines within Hermosa Street. Wastewater will discharge to the City's wastewater treatment plant and the Public Works department will regularly monitor the waste water discharge to meet City requirements. Stormwater will discharge into one of the six stormwater drainage basins within the City limits. The project would not significantly impact water supplies nor would it significantly impact a landfill. The project would comply with federal, state, and local statutes and regulations related to solid waste and is anticipated to recycle at least 50% of its solid waste per local policies.

## 7.0 MANDATORY FINDINGS OF SIGNIFICANCE

| MANDATORY FINDINGS OF SIGNIFICANCE   | Potentially Significant Impact | Less Than Significant with Mitigation | Less Than Significant Impact | No Impact                           |
|--|--------------------------------|---------------------------------------|------------------------------|-------------------------------------|
| a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?   | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |
| c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?  | <input type="checkbox"/>       | <input type="checkbox"/>              | <input type="checkbox"/>     | <input checked="" type="checkbox"/> |

### **Discussion:**

No impact. The project does not have the potential to degrade the quality of the environment, nor substantially reduce the habitat of a fish or wildlife species, nor cause a fish or wildlife population to drop below self-sustaining levels, nor threaten to eliminate a plant or animal community, nor reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory. The project does not have impacts that are individually limited, but cumulatively considerable. The project does not have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly.

## **8.0 MITIGATION MEASURES**

The following mitigation measures are identified for the proposed project.

### **Aesthetics**

AE 1: The project will incorporate standard light shielding measures for street light fixtures to mitigate any potential adverse glare impacts.

### **Air Quality**

AQ 1: The project shall be subject to all applicable mandatory air pollution control measures of the San Joaquin Valley Unified Air Pollution Control District in effect at time of development, including, but not limited to: Regulation VIII (Fugitive PM10 Prohibitions), Regulation VIII (Rules 8011-8081), Rule 4102 (Nuisance), 4103 (Open Burning), Rule 4601 (Architectural Coatings), Rule 4641 (Cutback, Slow Cure, and Emulsified Asphalt, Paving and Maintenance Operations), Rule 4901 (Wood Burning Fire Places and Wood Burning Heaters), and Rule 9510 (Indirect Source Review). The developer shall specifically demonstrate compliance with San Joaquin Valley Air Pollution Control District Rule 9510 (Indirect Source Review), including payment of all applicable fees, prior to the issuance of the first building permit.

### **Cultural Resources**

CR 1: Pursuant to CEQA Guidelines 15064.5 (f), provisions for historical or unique archaeological resources accidentally discovered during construction should be instituted. Therefore, in the event that any prehistoric or historic subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be halted and a qualified archaeologist or paleontologist shall be contacted to assess the significance of the find. If any find is determined to be significant, project proponents and the qualified archaeologist and/or paleontologist would meet to determine the appropriate avoidance measures or other appropriate mitigation. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and a report prepared by the qualified archaeologist according to current professional standards. If the discovery includes human remains, CEQA Guidelines 15064.5 (e)(1) shall be followed.

### **Noise**

NO 1: High noise levels resulting from construction activities shall be limited to the hours of 6:00 a.m. to 10:00 p.m, including weekends and holidays.

## 9.0 REFERENCES

City of Lindsay Municipal Code, Title 18: Zoning

San Joaquin Valley Air Pollution Control District. 2002. Guide For Assessing And Mitigating Air Quality Impacts

California Emissions Estimator Model Version: CalEEMod.2013.2.2

ITE Trip Generation Report (7<sup>th</sup> Edition)

Caltrans Letter, 6-TUL-65-29.53

**New Subway Restaurant (SPR 15-03)**  
**San Joaquin Valley Unified APCD Air District, Annual**

**1.0 Project Characteristics**

---

**1.1 Land Usage**

| Land Uses                            | Size | Metric   | Lot Acreage | Floor Surface Area | Population |
|--------------------------------------|------|----------|-------------|--------------------|------------|
| Fast Food Restaurant with Drive Thru | 2.00 | 1000sqft | 0.05        | 2,000.00           | 0          |

**1.2 Other Project Characteristics**

|                                 |                            |                                 |       |                                  |       |
|---------------------------------|----------------------------|---------------------------------|-------|----------------------------------|-------|
| <b>Urbanization</b>             | Urban                      | <b>Wind Speed (m/s)</b>         | 2.7   | <b>Precipitation Freq (Days)</b> | 45    |
| <b>Climate Zone</b>             | 7                          |                                 |       | <b>Operational Year</b>          | 2016  |
| <b>Utility Company</b>          | Southern California Edison |                                 |       |                                  |       |
| <b>CO2 Intensity (lb/MW hr)</b> | 630.89                     | <b>CH4 Intensity (lb/MW hr)</b> | 0.029 | <b>N2O Intensity (lb/MW hr)</b>  | 0.006 |

**1.3 User Entered Comments & Non-Default Data**

- Project Characteristics -
- Land Use -
- Construction Phase -
- Mobile Land Use Mitigation -
- Area Mitigation -
- Water Mitigation -
- Waste Mitigation -

| Table Name                | Column Name     | Default Value | New Value |
|---------------------------|-----------------|---------------|-----------|
| tblProjectCharacteristics | OperationalYear | 2014          | 2016      |

## 2.0 Emissions Summary

### 2.1 Overall Construction

#### Unmitigated Construction

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10      | Exhaust PM10  | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4           | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|--------------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|----------------|---------------|---------------|----------------|
| Year         | tons/yr       |               |               |                    |                    |               |               |                    |               |               | MT/yr         |                |                |               |               |                |
| 2015         | 0.1005        | 0.8338        | 0.5031        | 6.9000e-004        | 2.2800e-003        | 0.0580        | 0.0603        | 7.8000e-004        | 0.0536        | 0.0544        | 0.0000        | 65.3271        | 65.3271        | 0.0184        | 0.0000        | 65.7142        |
| <b>Total</b> | <b>0.1005</b> | <b>0.8338</b> | <b>0.5031</b> | <b>6.9000e-004</b> | <b>2.2800e-003</b> | <b>0.0580</b> | <b>0.0603</b> | <b>7.8000e-004</b> | <b>0.0536</b> | <b>0.0544</b> | <b>0.0000</b> | <b>65.3271</b> | <b>65.3271</b> | <b>0.0184</b> | <b>0.0000</b> | <b>65.7142</b> |

**Mitigated Construction**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10      | Exhaust PM10  | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4           | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|--------------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|----------------|---------------|---------------|----------------|
| Year         | tons/yr       |               |               |                    |                    |               |               |                    |               |               | MT/yr         |                |                |               |               |                |
| 2015         | 0.1005        | 0.8338        | 0.5031        | 6.9000e-004        | 2.2800e-003        | 0.0580        | 0.0603        | 7.8000e-004        | 0.0536        | 0.0544        | 0.0000        | 65.3271        | 65.3271        | 0.0184        | 0.0000        | 65.7141        |
| <b>Total</b> | <b>0.1005</b> | <b>0.8338</b> | <b>0.5031</b> | <b>6.9000e-004</b> | <b>2.2800e-003</b> | <b>0.0580</b> | <b>0.0603</b> | <b>7.8000e-004</b> | <b>0.0536</b> | <b>0.0544</b> | <b>0.0000</b> | <b>65.3271</b> | <b>65.3271</b> | <b>0.0184</b> | <b>0.0000</b> | <b>65.7141</b> |

|  | ROG | NOx | CO | SO2 | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4 | N2O | CO2e |
|--|-----|-----|----|-----|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|-----|-----|------|
|--|-----|-----|----|-----|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|-----|-----|------|

|                   |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |      |
|-------------------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|
| Percent Reduction | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 |
|-------------------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|

**2.2 Overall Operational**  
**Unmitigated Operational**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O                | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|--------------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |                    |                 |
| Area         | 9.2000e-003   | 0.0000        | 2.0000e-005   | 0.0000             |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 4.0000e-005     | 4.0000e-005     | 0.0000        | 0.0000             | 4.0000e-005     |
| Energy       | 1.0800e-003   | 9.8100e-003   | 8.2400e-003   | 6.0000e-005        |               | 7.5000e-004   | 7.5000e-004   |                | 7.5000e-004   | 7.5000e-004   | 0.0000        | 31.2045         | 31.2045         | 1.1500e-003   | 3.9000e-004        | 31.3498         |
| Mobile       | 0.8301        | 1.6558        | 7.9227        | 7.2400e-003        | 0.3802        | 0.0190        | 0.3991        | 0.1021         | 0.0174        | 0.1195        | 0.0000        | 588.3706        | 588.3706        | 0.0236        | 0.0000             | 588.8658        |
| Waste        |               |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 4.6769        | 0.0000          | 4.6769          | 0.2764        | 0.0000             | 10.4813         |
| Water        |               |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.1926        | 0.9788          | 1.1714          | 0.0198        | 4.8000e-004        | 1.7355          |
| <b>Total</b> | <b>0.8403</b> | <b>1.6656</b> | <b>7.9309</b> | <b>7.3000e-003</b> | <b>0.3802</b> | <b>0.0197</b> | <b>0.3999</b> | <b>0.1021</b>  | <b>0.0182</b> | <b>0.1203</b> | <b>4.8695</b> | <b>620.5539</b> | <b>625.4234</b> | <b>0.3210</b> | <b>8.7000e-004</b> | <b>632.4324</b> |

**Mitigated Operational**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O                | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|--------------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |                    |                 |
| Area         | 8.6200e-003   | 0.0000        | 2.0000e-005   | 0.0000             |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 4.0000e-005     | 4.0000e-005     | 0.0000        | 0.0000             | 4.0000e-005     |
| Energy       | 1.0800e-003   | 9.8100e-003   | 8.2400e-003   | 6.0000e-005        |               | 7.5000e-004   | 7.5000e-004   |                | 7.5000e-004   | 7.5000e-004   | 0.0000        | 31.2045         | 31.2045         | 1.1500e-003   | 3.9000e-004        | 31.3498         |
| Mobile       | 0.8301        | 1.6558        | 7.9227        | 7.2400e-003        | 0.3802        | 0.0190        | 0.3991        | 0.1021         | 0.0174        | 0.1195        | 0.0000        | 588.3706        | 588.3706        | 0.0236        | 0.0000             | 588.8658        |
| Waste        |               |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 2.3385        | 0.0000          | 2.3385          | 0.1382        | 0.0000             | 5.2406          |
| Water        |               |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.1626        | 0.7289          | 0.8914          | 0.0167        | 4.0000e-004        | 1.3668          |
| <b>Total</b> | <b>0.8398</b> | <b>1.6656</b> | <b>7.9309</b> | <b>7.3000e-003</b> | <b>0.3802</b> | <b>0.0197</b> | <b>0.3999</b> | <b>0.1021</b>  | <b>0.0182</b> | <b>0.1203</b> | <b>2.5010</b> | <b>620.3040</b> | <b>622.8050</b> | <b>0.1797</b> | <b>7.9000e-004</b> | <b>626.8231</b> |

|                   | ROG  | NOx  | CO   | SO2  | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio-CO2 | Total CO2 | CH4   | N2O  | CO2e |
|-------------------|------|------|------|------|---------------|--------------|------------|----------------|---------------|-------------|----------|----------|-----------|-------|------|------|
| Percent Reduction | 0.07 | 0.00 | 0.00 | 0.00 | 0.00          | 0.00         | 0.00       | 0.00           | 0.00          | 0.00        | 48.64    | 0.04     | 0.42      | 44.02 | 9.20 | 0.89 |

### 3.0 Construction Detail

#### Construction Phase

| Phase Number | Phase Name            | Phase Type            | Start Date | End Date   | Num Days Week | Num Days | Phase Description |
|--------------|-----------------------|-----------------------|------------|------------|---------------|----------|-------------------|
| 1            | Demolition            | Demolition            | 6/1/2015   | 6/12/2015  | 5             | 10       |                   |
| 2            | Site Preparation      | Site Preparation      | 6/13/2015  | 6/15/2015  | 5             | 1        |                   |
| 3            | Grading               | Grading               | 6/16/2015  | 6/17/2015  | 5             | 2        |                   |
| 4            | Building Construction | Building Construction | 6/18/2015  | 11/4/2015  | 5             | 100      |                   |
| 5            | Paving                | Paving                | 11/5/2015  | 11/11/2015 | 5             | 5        |                   |
| 6            | Architectural Coating | Architectural Coating | 11/12/2015 | 11/18/2015 | 5             | 5        |                   |

Acres of Grading (Site Preparation Phase): 0.5

Acres of Grading (Grading Phase): 0

Acres of Paving: 0

Residential Indoor: 0; Residential Outdoor: 0; Non-Residential Indoor: 3,000; Non-Residential Outdoor: 1,000 (Architectural Coating – sqft)

#### OffRoad Equipment

| Phase Name            | Offroad Equipment Type   | Amount | Usage Hours | Horse Power | Load Factor |
|-----------------------|--------------------------|--------|-------------|-------------|-------------|
| Architectural Coating | Air Compressors          | 1      | 6.00        | 78          | 0.48        |
| Paving                | Cement and Mortar Mixers | 4      | 6.00        | 9           | 0.56        |

|                       |                           |   |      |     |      |
|-----------------------|---------------------------|---|------|-----|------|
| Demolition            | Concrete/Industrial Saws  | 1 | 8.00 | 81  | 0.73 |
| Grading               | Concrete/Industrial Saws  | 1 | 8.00 | 81  | 0.73 |
| Building Construction | Cranes                    | 1 | 4.00 | 226 | 0.29 |
| Building Construction | Forklifts                 | 2 | 6.00 | 89  | 0.20 |
| Site Preparation      | Graders                   | 1 | 8.00 | 174 | 0.41 |
| Paving                | Pavers                    | 1 | 7.00 | 125 | 0.42 |
| Paving                | Rollers                   | 1 | 7.00 | 80  | 0.38 |
| Demolition            | Rubber Tired Dozers       | 1 | 1.00 | 255 | 0.40 |
| Grading               | Rubber Tired Dozers       | 1 | 1.00 | 255 | 0.40 |
| Building Construction | Tractors/Loaders/Backhoes | 2 | 8.00 | 97  | 0.37 |
| Demolition            | Tractors/Loaders/Backhoes | 2 | 6.00 | 97  | 0.37 |
| Grading               | Tractors/Loaders/Backhoes | 2 | 6.00 | 97  | 0.37 |
| Paving                | Tractors/Loaders/Backhoes | 1 | 7.00 | 97  | 0.37 |
| Site Preparation      | Tractors/Loaders/Backhoes | 1 | 8.00 | 97  | 0.37 |

**Trips and VMT**

| Phase Name            | Offroad Equipment Count | Worker Trip Number | Vendor Trip Number | Hauling Trip Number | Worker Trip Length | Vendor Trip Length | Hauling Trip Length | Worker Vehicle Class | Vendor Vehicle Class | Hauling Vehicle Class |
|-----------------------|-------------------------|--------------------|--------------------|---------------------|--------------------|--------------------|---------------------|----------------------|----------------------|-----------------------|
| Demolition            | 4                       | 10.00              | 0.00               | 0.00                | 10.80              | 7.30               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Site Preparation      | 2                       | 5.00               | 0.00               | 0.00                | 10.80              | 7.30               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Grading               | 4                       | 10.00              | 0.00               | 0.00                | 10.80              | 7.30               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Building Construction | 5                       | 1.00               | 0.00               | 0.00                | 10.80              | 7.30               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Paving                | 7                       | 18.00              | 0.00               | 0.00                | 10.80              | 7.30               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Architectural Coating | 1                       | 0.00               | 0.00               | 0.00                | 10.80              | 7.30               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |

### 3.1 Mitigation Measures Construction

### 3.2 Demolition - 2015

#### Unmitigated Construction On-Site

|              | ROG                | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |               |               |                    |               |               |
| Off-Road     | 7.0600e-003        | 0.0597        | 0.0441        | 6.0000e-005        |               | 4.3700e-003        | 4.3700e-003        |                | 4.1800e-003        | 4.1800e-003        | 0.0000        | 5.4460        | 5.4460        | 1.1100e-003        | 0.0000        | 5.4694        |
| <b>Total</b> | <b>7.0600e-003</b> | <b>0.0597</b> | <b>0.0441</b> | <b>6.0000e-005</b> |               | <b>4.3700e-003</b> | <b>4.3700e-003</b> |                | <b>4.1800e-003</b> | <b>4.1800e-003</b> | <b>0.0000</b> | <b>5.4460</b> | <b>5.4460</b> | <b>1.1100e-003</b> | <b>0.0000</b> | <b>5.4694</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.2000e-004        | 2.7000e-004        | 2.6900e-003        | 0.0000        | 4.0000e-004        | 0.0000        | 4.0000e-004        | 1.1000e-004        | 0.0000        | 1.1000e-004        | 0.0000        | 0.3721        | 0.3721        | 2.0000e-005        | 0.0000        | 0.3726        |
| <b>Total</b> | <b>2.2000e-004</b> | <b>2.7000e-004</b> | <b>2.6900e-003</b> | <b>0.0000</b> | <b>4.0000e-004</b> | <b>0.0000</b> | <b>4.0000e-004</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>0.3721</b> | <b>0.3721</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.3726</b> |

**Mitigated Construction On-Site**

|              | ROG                | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |               |               |                    |               |               |
| Off-Road     | 7.0600e-003        | 0.0597        | 0.0441        | 6.0000e-005        |               | 4.3700e-003        | 4.3700e-003        |                | 4.1800e-003        | 4.1800e-003        | 0.0000        | 5.4460        | 5.4460        | 1.1100e-003        | 0.0000        | 5.4694        |
| <b>Total</b> | <b>7.0600e-003</b> | <b>0.0597</b> | <b>0.0441</b> | <b>6.0000e-005</b> |               | <b>4.3700e-003</b> | <b>4.3700e-003</b> |                | <b>4.1800e-003</b> | <b>4.1800e-003</b> | <b>0.0000</b> | <b>5.4460</b> | <b>5.4460</b> | <b>1.1100e-003</b> | <b>0.0000</b> | <b>5.4694</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.2000e-004        | 2.7000e-004        | 2.6900e-003        | 0.0000        | 4.0000e-004        | 0.0000        | 4.0000e-004        | 1.1000e-004        | 0.0000        | 1.1000e-004        | 0.0000        | 0.3721        | 0.3721        | 2.0000e-005        | 0.0000        | 0.3726        |
| <b>Total</b> | <b>2.2000e-004</b> | <b>2.7000e-004</b> | <b>2.6900e-003</b> | <b>0.0000</b> | <b>4.0000e-004</b> | <b>0.0000</b> | <b>4.0000e-004</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>0.3721</b> | <b>0.3721</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.3726</b> |

**3.3 Site Preparation - 2015**

**Unmitigated Construction On-Site**

|               | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|---------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category      | tons/yr            |                    |                    |               |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Fugitive Dust |                    |                    |                    |               | 2.7000e-004        | 0.0000             | 2.7000e-004        | 3.0000e-005        | 0.0000             | 3.0000e-005        | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Off-Road      | 7.1000e-004        | 7.1500e-003        | 3.7000e-003        | 0.0000        |                    | 4.4000e-004        | 4.4000e-004        |                    | 4.0000e-004        | 4.0000e-004        | 0.0000        | 0.4466        | 0.4466        | 1.3000e-004        | 0.0000        | 0.4494        |
| <b>Total</b>  | <b>7.1000e-004</b> | <b>7.1500e-003</b> | <b>3.7000e-003</b> | <b>0.0000</b> | <b>2.7000e-004</b> | <b>4.4000e-004</b> | <b>7.1000e-004</b> | <b>3.0000e-005</b> | <b>4.0000e-004</b> | <b>4.3000e-004</b> | <b>0.0000</b> | <b>0.4466</b> | <b>0.4466</b> | <b>1.3000e-004</b> | <b>0.0000</b> | <b>0.4494</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4           | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|---------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |               |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Worker       | 1.0000e-005        | 1.0000e-005        | 1.3000e-004        | 0.0000        | 2.0000e-005        | 0.0000        | 2.0000e-005        | 1.0000e-005        | 0.0000        | 1.0000e-005        | 0.0000        | 0.0186        | 0.0186        | 0.0000        | 0.0000        | 0.0186        |
| <b>Total</b> | <b>1.0000e-005</b> | <b>1.0000e-005</b> | <b>1.3000e-004</b> | <b>0.0000</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>2.0000e-005</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>0.0186</b> | <b>0.0186</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0186</b> |

**Mitigated Construction On-Site**

|               | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|---------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category      | tons/yr            |                    |                    |               |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Fugitive Dust |                    |                    |                    |               | 2.7000e-004        | 0.0000             | 2.7000e-004        | 3.0000e-005        | 0.0000             | 3.0000e-005        | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Off-Road      | 7.1000e-004        | 7.1500e-003        | 3.7000e-003        | 0.0000        |                    | 4.4000e-004        | 4.4000e-004        |                    | 4.0000e-004        | 4.0000e-004        | 0.0000        | 0.4466        | 0.4466        | 1.3000e-004        | 0.0000        | 0.4494        |
| <b>Total</b>  | <b>7.1000e-004</b> | <b>7.1500e-003</b> | <b>3.7000e-003</b> | <b>0.0000</b> | <b>2.7000e-004</b> | <b>4.4000e-004</b> | <b>7.1000e-004</b> | <b>3.0000e-005</b> | <b>4.0000e-004</b> | <b>4.3000e-004</b> | <b>0.0000</b> | <b>0.4466</b> | <b>0.4466</b> | <b>1.3000e-004</b> | <b>0.0000</b> | <b>0.4494</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4           | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|---------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |               |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Worker       | 1.0000e-005        | 1.0000e-005        | 1.3000e-004        | 0.0000        | 2.0000e-005        | 0.0000        | 2.0000e-005        | 1.0000e-005        | 0.0000        | 1.0000e-005        | 0.0000        | 0.0186        | 0.0186        | 0.0000        | 0.0000        | 0.0186        |
| <b>Total</b> | <b>1.0000e-005</b> | <b>1.0000e-005</b> | <b>1.3000e-004</b> | <b>0.0000</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>2.0000e-005</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>0.0186</b> | <b>0.0186</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0186</b> |

**3.4 Grading - 2015**

**Unmitigated Construction On-Site**

|               | ROG                | NOx           | CO                 | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|---------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category      | tons/yr            |               |                    |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Fugitive Dust |                    |               |                    |                    | 7.5000e-004        | 0.0000             | 7.5000e-004        | 4.1000e-004        | 0.0000             | 4.1000e-004        | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Off-Road      | 1.4100e-003        | 0.0119        | 8.8100e-003        | 1.0000e-005        |                    | 8.7000e-004        | 8.7000e-004        |                    | 8.4000e-004        | 8.4000e-004        | 0.0000        | 1.0892        | 1.0892        | 2.2000e-004        | 0.0000        | 1.0939        |
| <b>Total</b>  | <b>1.4100e-003</b> | <b>0.0119</b> | <b>8.8100e-003</b> | <b>1.0000e-005</b> | <b>7.5000e-004</b> | <b>8.7000e-004</b> | <b>1.6200e-003</b> | <b>4.1000e-004</b> | <b>8.4000e-004</b> | <b>1.2500e-003</b> | <b>0.0000</b> | <b>1.0892</b> | <b>1.0892</b> | <b>2.2000e-004</b> | <b>0.0000</b> | <b>1.0939</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4           | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|---------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |               |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Worker       | 4.0000e-005        | 5.0000e-005        | 5.4000e-004        | 0.0000        | 8.0000e-005        | 0.0000        | 8.0000e-005        | 2.0000e-005        | 0.0000        | 2.0000e-005        | 0.0000        | 0.0744        | 0.0744        | 0.0000        | 0.0000        | 0.0745        |
| <b>Total</b> | <b>4.0000e-005</b> | <b>5.0000e-005</b> | <b>5.4000e-004</b> | <b>0.0000</b> | <b>8.0000e-005</b> | <b>0.0000</b> | <b>8.0000e-005</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.0744</b> | <b>0.0744</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0745</b> |

**Mitigated Construction On-Site**

|               | ROG                | NOx           | CO                 | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|---------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category      | tons/yr            |               |                    |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Fugitive Dust |                    |               |                    |                    | 7.5000e-004        | 0.0000             | 7.5000e-004        | 4.1000e-004        | 0.0000             | 4.1000e-004        | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Off-Road      | 1.4100e-003        | 0.0119        | 8.8100e-003        | 1.0000e-005        |                    | 8.7000e-004        | 8.7000e-004        |                    | 8.4000e-004        | 8.4000e-004        | 0.0000        | 1.0892        | 1.0892        | 2.2000e-004        | 0.0000        | 1.0939        |
| <b>Total</b>  | <b>1.4100e-003</b> | <b>0.0119</b> | <b>8.8100e-003</b> | <b>1.0000e-005</b> | <b>7.5000e-004</b> | <b>8.7000e-004</b> | <b>1.6200e-003</b> | <b>4.1000e-004</b> | <b>8.4000e-004</b> | <b>1.2500e-003</b> | <b>0.0000</b> | <b>1.0892</b> | <b>1.0892</b> | <b>2.2000e-004</b> | <b>0.0000</b> | <b>1.0939</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4           | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|---------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |               |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Worker       | 4.0000e-005        | 5.0000e-005        | 5.4000e-004        | 0.0000        | 8.0000e-005        | 0.0000        | 8.0000e-005        | 2.0000e-005        | 0.0000        | 2.0000e-005        | 0.0000        | 0.0744        | 0.0744        | 0.0000        | 0.0000        | 0.0745        |
| <b>Total</b> | <b>4.0000e-005</b> | <b>5.0000e-005</b> | <b>5.4000e-004</b> | <b>0.0000</b> | <b>8.0000e-005</b> | <b>0.0000</b> | <b>8.0000e-005</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.0744</b> | <b>0.0744</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0745</b> |

**3.5 Building Construction - 2015**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4           | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|---------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |               |               |                |
| Off-Road     | 0.0727        | 0.7189        | 0.4149        | 5.7000e-004        |               | 0.0500        | 0.0500        |                | 0.0460        | 0.0460        | 0.0000        | 54.0547        | 54.0547        | 0.0161        | 0.0000        | 54.3936        |
| <b>Total</b> | <b>0.0727</b> | <b>0.7189</b> | <b>0.4149</b> | <b>5.7000e-004</b> |               | <b>0.0500</b> | <b>0.0500</b> |                | <b>0.0460</b> | <b>0.0460</b> | <b>0.0000</b> | <b>54.0547</b> | <b>54.0547</b> | <b>0.0161</b> | <b>0.0000</b> | <b>54.3936</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.2000e-004        | 2.7000e-004        | 2.6900e-003        | 0.0000        | 4.0000e-004        | 0.0000        | 4.0000e-004        | 1.1000e-004        | 0.0000        | 1.1000e-004        | 0.0000        | 0.3721        | 0.3721        | 2.0000e-005        | 0.0000        | 0.3726        |
| <b>Total</b> | <b>2.2000e-004</b> | <b>2.7000e-004</b> | <b>2.6900e-003</b> | <b>0.0000</b> | <b>4.0000e-004</b> | <b>0.0000</b> | <b>4.0000e-004</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>0.3721</b> | <b>0.3721</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.3726</b> |

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4           | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|---------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |               |               |                |
| Off-Road     | 0.0727        | 0.7189        | 0.4149        | 5.7000e-004        |               | 0.0500        | 0.0500        |                | 0.0460        | 0.0460        | 0.0000        | 54.0546        | 54.0546        | 0.0161        | 0.0000        | 54.3935        |
| <b>Total</b> | <b>0.0727</b> | <b>0.7189</b> | <b>0.4149</b> | <b>5.7000e-004</b> |               | <b>0.0500</b> | <b>0.0500</b> |                | <b>0.0460</b> | <b>0.0460</b> | <b>0.0000</b> | <b>54.0546</b> | <b>54.0546</b> | <b>0.0161</b> | <b>0.0000</b> | <b>54.3935</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.2000e-004        | 2.7000e-004        | 2.6900e-003        | 0.0000        | 4.0000e-004        | 0.0000        | 4.0000e-004        | 1.1000e-004        | 0.0000        | 1.1000e-004        | 0.0000        | 0.3721        | 0.3721        | 2.0000e-005        | 0.0000        | 0.3726        |
| <b>Total</b> | <b>2.2000e-004</b> | <b>2.7000e-004</b> | <b>2.6900e-003</b> | <b>0.0000</b> | <b>4.0000e-004</b> | <b>0.0000</b> | <b>4.0000e-004</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>0.3721</b> | <b>0.3721</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.3726</b> |

**3.6 Paving - 2015**

**Unmitigated Construction On-Site**

|              | ROG                | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |               |               |                    |               |               |
| Off-Road     | 3.0200e-003        | 0.0289        | 0.0184        | 3.0000e-005        |               | 1.8100e-003        | 1.8100e-003        |                | 1.6800e-003        | 1.6800e-003        | 0.0000        | 2.4801        | 2.4801        | 6.7000e-004        | 0.0000        | 2.4943        |
| Paving       | 0.0000             |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| <b>Total</b> | <b>3.0200e-003</b> | <b>0.0289</b> | <b>0.0184</b> | <b>3.0000e-005</b> |               | <b>1.8100e-003</b> | <b>1.8100e-003</b> |                | <b>1.6800e-003</b> | <b>1.6800e-003</b> | <b>0.0000</b> | <b>2.4801</b> | <b>2.4801</b> | <b>6.7000e-004</b> | <b>0.0000</b> | <b>2.4943</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 1.9000e-004        | 2.4000e-004        | 2.4200e-003        | 0.0000        | 3.6000e-004        | 0.0000        | 3.6000e-004        | 1.0000e-004        | 0.0000        | 1.0000e-004        | 0.0000        | 0.3349        | 0.3349        | 2.0000e-005        | 0.0000        | 0.3353        |
| <b>Total</b> | <b>1.9000e-004</b> | <b>2.4000e-004</b> | <b>2.4200e-003</b> | <b>0.0000</b> | <b>3.6000e-004</b> | <b>0.0000</b> | <b>3.6000e-004</b> | <b>1.0000e-004</b> | <b>0.0000</b> | <b>1.0000e-004</b> | <b>0.0000</b> | <b>0.3349</b> | <b>0.3349</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.3353</b> |

**Mitigated Construction On-Site**

|              | ROG                | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |               |               |                    |               |               |
| Off-Road     | 3.0200e-003        | 0.0289        | 0.0184        | 3.0000e-005        |               | 1.8100e-003        | 1.8100e-003        |                | 1.6800e-003        | 1.6800e-003        | 0.0000        | 2.4801        | 2.4801        | 6.7000e-004        | 0.0000        | 2.4943        |
| Paving       | 0.0000             |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| <b>Total</b> | <b>3.0200e-003</b> | <b>0.0289</b> | <b>0.0184</b> | <b>3.0000e-005</b> |               | <b>1.8100e-003</b> | <b>1.8100e-003</b> |                | <b>1.6800e-003</b> | <b>1.6800e-003</b> | <b>0.0000</b> | <b>2.4801</b> | <b>2.4801</b> | <b>6.7000e-004</b> | <b>0.0000</b> | <b>2.4943</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2           | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |               |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 1.9000e-004        | 2.4000e-004        | 2.4200e-003        | 0.0000        | 3.6000e-004        | 0.0000        | 3.6000e-004        | 1.0000e-004        | 0.0000        | 1.0000e-004        | 0.0000        | 0.3349        | 0.3349        | 2.0000e-005        | 0.0000        | 0.3353        |
| <b>Total</b> | <b>1.9000e-004</b> | <b>2.4000e-004</b> | <b>2.4200e-003</b> | <b>0.0000</b> | <b>3.6000e-004</b> | <b>0.0000</b> | <b>3.6000e-004</b> | <b>1.0000e-004</b> | <b>0.0000</b> | <b>1.0000e-004</b> | <b>0.0000</b> | <b>0.3349</b> | <b>0.3349</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.3353</b> |

**3.7 Architectural Coating - 2015**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx                | CO                 | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|-----------------|---------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category        | tons/yr       |                    |                    |                    |               |                    |                    |                |                    |                    | MT/yr         |               |               |                    |               |               |
| Archit. Coating | 0.0139        |                    |                    |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Off-Road        | 1.0200e-003   | 6.4300e-003        | 4.7500e-003        | 1.0000e-005        |               | 5.5000e-004        | 5.5000e-004        |                | 5.5000e-004        | 5.5000e-004        | 0.0000        | 0.6383        | 0.6383        | 8.0000e-005        | 0.0000        | 0.6401        |
| <b>Total</b>    | <b>0.0149</b> | <b>6.4300e-003</b> | <b>4.7500e-003</b> | <b>1.0000e-005</b> |               | <b>5.5000e-004</b> | <b>5.5000e-004</b> |                | <b>5.5000e-004</b> | <b>5.5000e-004</b> | <b>0.0000</b> | <b>0.6383</b> | <b>0.6383</b> | <b>8.0000e-005</b> | <b>0.0000</b> | <b>0.6401</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2     | Total CO2     | CH4           | N2O           | CO2e          |
|--------------|---------------|---------------|---------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|---------------|---------------|---------------|---------------|---------------|
| Category     | tons/yr       |               |               |               |               |               |               |                |               |               | MT/yr         |               |               |               |               |               |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Worker       | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| <b>Total</b> | <b>0.0000</b>  | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> |

**Mitigated Construction On-Site**

|                 | ROG           | NOx                | CO                 | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|-----------------|---------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category        | tons/yr       |                    |                    |                    |               |                    |                    |                |                    |                    | MT/yr         |               |               |                    |               |               |
| Archit. Coating | 0.0139        |                    |                    |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Off-Road        | 1.0200e-003   | 6.4300e-003        | 4.7500e-003        | 1.0000e-005        |               | 5.5000e-004        | 5.5000e-004        |                | 5.5000e-004        | 5.5000e-004        | 0.0000        | 0.6383        | 0.6383        | 8.0000e-005        | 0.0000        | 0.6401        |
| <b>Total</b>    | <b>0.0149</b> | <b>6.4300e-003</b> | <b>4.7500e-003</b> | <b>1.0000e-005</b> |               | <b>5.5000e-004</b> | <b>5.5000e-004</b> |                | <b>5.5000e-004</b> | <b>5.5000e-004</b> | <b>0.0000</b> | <b>0.6383</b> | <b>0.6383</b> | <b>8.0000e-005</b> | <b>0.0000</b> | <b>0.6401</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2     | Total CO2     | CH4           | N2O           | CO2e          |
|--------------|---------------|---------------|---------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|---------------|---------------|---------------|---------------|---------------|
| Category     | tons/yr       |               |               |               |               |               |               |                |               |               | MT/yr         |               |               |               |               |               |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| Worker       | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        | 0.0000        |
| <b>Total</b> | <b>0.0000</b>  | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> |

**4.0 Operational Detail - Mobile**

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**4.1 Mitigation Measures Mobile**

|             | ROG     | NOx    | CO     | SO2         | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4    | N2O    | CO2e     |
|-------------|---------|--------|--------|-------------|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|--------|--------|----------|
| Category    | tons/yr |        |        |             |               |              |            |                |               |             | MT/yr    |           |           |        |        |          |
| Mitigated   | 0.8301  | 1.6558 | 7.9227 | 7.2400e-003 | 0.3802        | 0.0190       | 0.3991     | 0.1021         | 0.0174        | 0.1195      | 0.0000   | 588.3706  | 588.3706  | 0.0236 | 0.0000 | 588.8658 |
| Unmitigated | 0.8301  | 1.6558 | 7.9227 | 7.2400e-003 | 0.3802        | 0.0190       | 0.3991     | 0.1021         | 0.0174        | 0.1195      | 0.0000   | 588.3706  | 588.3706  | 0.0236 | 0.0000 | 588.8658 |

#### 4.2 Trip Summary Information

| Land Use                             | Average Daily Trip Rate |          |          | Unmitigated | Mitigated  |
|--------------------------------------|-------------------------|----------|----------|-------------|------------|
|                                      | Weekday                 | Saturday | Sunday   | Annual VMT  | Annual VMT |
| Fast Food Restaurant with Drive Thru | 992.24                  | 1,444.06 | 1085.44  | 999,821     | 999,821    |
| Total                                | 992.24                  | 1,444.06 | 1,085.44 | 999,821     | 999,821    |

#### 4.3 Trip Type Information

| Land Use                        | Miles      |            |             | Trip %     |            |             | Trip Purpose % |          |         |
|---------------------------------|------------|------------|-------------|------------|------------|-------------|----------------|----------|---------|
|                                 | H-W or C-W | H-S or C-C | H-O or C-NW | H-W or C-W | H-S or C-C | H-O or C-NW | Primary        | Diverted | Pass-by |
| Fast Food Restaurant with Drive | 9.50       | 7.30       | 7.30        | 2.20       | 78.80      | 19.00       | 29             | 21       | 50      |

| LDA      | LDT1     | LDT2     | MDV      | LHD1     | LHD2     | MHD      | HHD      | OBUS     | UBUS     | MCY      | SBUS     | MH       |
|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| 0.413014 | 0.062673 | 0.156172 | 0.176687 | 0.051255 | 0.007895 | 0.018867 | 0.100331 | 0.001803 | 0.001598 | 0.006448 | 0.000946 | 0.002310 |

#### 5.0 Energy Detail

**4.4 Fleet Mix**

Historical Energy Use: N

**5.1 Mitigation Measures Energy**

|                         | ROG         | NOx         | CO          | SO2         | Fugitive PM10 | Exhaust PM10 | PM10 Total  | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4         | N2O         | CO2e    |
|-------------------------|-------------|-------------|-------------|-------------|---------------|--------------|-------------|----------------|---------------|-------------|----------|-----------|-----------|-------------|-------------|---------|
| Category                | tons/yr     |             |             |             |               |              |             |                |               |             | MT/yr    |           |           |             |             |         |
| Electricity Mitigated   |             |             |             |             |               | 0.0000       | 0.0000      |                | 0.0000        | 0.0000      | 0.0000   | 20.5296   | 20.5296   | 9.4000e-004 | 2.0000e-004 | 20.6100 |
| Electricity Unmitigated |             |             |             |             |               | 0.0000       | 0.0000      |                | 0.0000        | 0.0000      | 0.0000   | 20.5296   | 20.5296   | 9.4000e-004 | 2.0000e-004 | 20.6100 |
| NaturalGas Mitigated    | 1.0800e-003 | 9.8100e-003 | 8.2400e-003 | 6.0000e-005 |               | 7.5000e-004  | 7.5000e-004 |                | 7.5000e-004   | 7.5000e-004 | 0.0000   | 10.6749   | 10.6749   | 2.0000e-004 | 2.0000e-004 | 10.7399 |
| NaturalGas Unmitigated  | 1.0800e-003 | 9.8100e-003 | 8.2400e-003 | 6.0000e-005 |               | 7.5000e-004  | 7.5000e-004 |                | 7.5000e-004   | 7.5000e-004 | 0.0000   | 10.6749   | 10.6749   | 2.0000e-004 | 2.0000e-004 | 10.7399 |

### 5.2 Energy by Land Use - NaturalGas

#### Unmitigated

|                                      | NaturalGas Use | ROG                | NOx                | CO                 | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O                | CO2e           |
|--------------------------------------|----------------|--------------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|--------------------|----------------|
| Land Use                             | kBTU/yr        | tons/yr            |                    |                    |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |                    |                |
| Fast Food Restaurant with Drive Thru | 200040         | 1.0800e-003        | 9.8100e-003        | 8.2400e-003        | 6.0000e-005        |               | 7.5000e-004        | 7.5000e-004        |                | 7.5000e-004        | 7.5000e-004        | 0.0000        | 10.6749        | 10.6749        | 2.0000e-004        | 2.0000e-004        | 10.7399        |
| <b>Total</b>                         |                | <b>1.0800e-003</b> | <b>9.8100e-003</b> | <b>8.2400e-003</b> | <b>6.0000e-005</b> |               | <b>7.5000e-004</b> | <b>7.5000e-004</b> |                | <b>7.5000e-004</b> | <b>7.5000e-004</b> | <b>0.0000</b> | <b>10.6749</b> | <b>10.6749</b> | <b>2.0000e-004</b> | <b>2.0000e-004</b> | <b>10.7399</b> |

#### Mitigated

|                                      | NaturalGas Use | ROG                | NOx                | CO                 | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O                | CO2e           |
|--------------------------------------|----------------|--------------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|--------------------|----------------|
| Land Use                             | kBTU/yr        | tons/yr            |                    |                    |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |                    |                |
| Fast Food Restaurant with Drive Thru | 200040         | 1.0800e-003        | 9.8100e-003        | 8.2400e-003        | 6.0000e-005        |               | 7.5000e-004        | 7.5000e-004        |                | 7.5000e-004        | 7.5000e-004        | 0.0000        | 10.6749        | 10.6749        | 2.0000e-004        | 2.0000e-004        | 10.7399        |
| <b>Total</b>                         |                | <b>1.0800e-003</b> | <b>9.8100e-003</b> | <b>8.2400e-003</b> | <b>6.0000e-005</b> |               | <b>7.5000e-004</b> | <b>7.5000e-004</b> |                | <b>7.5000e-004</b> | <b>7.5000e-004</b> | <b>0.0000</b> | <b>10.6749</b> | <b>10.6749</b> | <b>2.0000e-004</b> | <b>2.0000e-004</b> | <b>10.7399</b> |

### 5.3 Energy by Land Use - Electricity

#### Unmitigated

|                                 | Electricity Use |         | Total CO2      | CH4                | N2O                | CO2e           |
|---------------------------------|-----------------|---------|----------------|--------------------|--------------------|----------------|
| Land Use                        | kWh/yr          | tons/yr | MT/yr          |                    |                    |                |
| Fast Food Restaurant with Drive | 71740           |         | 20.5296        | 9.4000e-004        | 2.0000e-004        | 20.6100        |
| <b>Total</b>                    |                 |         | <b>20.5296</b> | <b>9.4000e-004</b> | <b>2.0000e-004</b> | <b>20.6100</b> |

#### Mitigated

|                                 | Electricity Use |         | Total CO2      | CH4                | N2O                | CO2e           |
|---------------------------------|-----------------|---------|----------------|--------------------|--------------------|----------------|
| Land Use                        | kWh/yr          | tons/yr | MT/yr          |                    |                    |                |
| Fast Food Restaurant with Drive | 71740           |         | 20.5296        | 9.4000e-004        | 2.0000e-004        | 20.6100        |
| <b>Total</b>                    |                 |         | <b>20.5296</b> | <b>9.4000e-004</b> | <b>2.0000e-004</b> | <b>20.6100</b> |

### 6.0 Area Detail

### 6.1 Mitigation Measures Area

Use Low VOC Paint - Non-Residential Interior

Use Low VOC Paint - Non-Residential Exterior

No Hearths Installed

Use Low VOC Cleaning Supplies

|             | ROG         | NOx    | CO          | SO2    | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2   | Total CO2   | CH4    | N2O    | CO2e        |
|-------------|-------------|--------|-------------|--------|---------------|--------------|------------|----------------|---------------|-------------|----------|-------------|-------------|--------|--------|-------------|
| Category    | tons/yr     |        |             |        |               |              |            |                |               |             | MT/yr    |             |             |        |        |             |
| Mitigated   | 8.6200e-003 | 0.0000 | 2.0000e-005 | 0.0000 |               | 0.0000       | 0.0000     |                | 0.0000        | 0.0000      | 0.0000   | 4.0000e-005 | 4.0000e-005 | 0.0000 | 0.0000 | 4.0000e-005 |
| Unmitigated | 9.2000e-003 | 0.0000 | 2.0000e-005 | 0.0000 |               | 0.0000       | 0.0000     |                | 0.0000        | 0.0000      | 0.0000   | 4.0000e-005 | 4.0000e-005 | 0.0000 | 0.0000 | 4.0000e-005 |

### 6.2 Area by SubCategory

#### Unmitigated

|                       | ROG                | NOx           | CO                 | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2          | Total CO2          | CH4           | N2O           | CO2e               |
|-----------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|--------------------|--------------------|---------------|---------------|--------------------|
| SubCategory           | tons/yr            |               |                    |               |               |               |               |                |               |               | MT/yr         |                    |                    |               |               |                    |
| Architectural Coating | 1.3900e-003        |               |                    |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000             |
| Consumer Products     | 7.8100e-003        |               |                    |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000             |
| Landscaping           | 0.0000             | 0.0000        | 2.0000e-005        | 0.0000        |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 4.0000e-005        | 4.0000e-005        | 0.0000        | 0.0000        | 4.0000e-005        |
| <b>Total</b>          | <b>9.2000e-003</b> | <b>0.0000</b> | <b>2.0000e-005</b> | <b>0.0000</b> |               | <b>0.0000</b> | <b>0.0000</b> |                | <b>0.0000</b> | <b>0.0000</b> | <b>0.0000</b> | <b>4.0000e-005</b> | <b>4.0000e-005</b> | <b>0.0000</b> | <b>0.0000</b> | <b>4.0000e-005</b> |

#### Mitigated

|                       | ROG         | NOx    | CO          | SO2    | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2   | Total CO2   | CH4    | N2O    | CO2e        |
|-----------------------|-------------|--------|-------------|--------|---------------|--------------|------------|----------------|---------------|-------------|----------|-------------|-------------|--------|--------|-------------|
| SubCategory           | tons/yr     |        |             |        |               |              |            |                |               |             | MT/yr    |             |             |        |        |             |
| Architectural Coating | 1.3900e-003 |        |             |        |               | 0.0000       | 0.0000     |                | 0.0000        | 0.0000      | 0.0000   | 0.0000      | 0.0000      | 0.0000 | 0.0000 | 0.0000      |
| Consumer Products     | 7.2300e-003 |        |             |        |               | 0.0000       | 0.0000     |                | 0.0000        | 0.0000      | 0.0000   | 0.0000      | 0.0000      | 0.0000 | 0.0000 | 0.0000      |
| Landscaping           | 0.0000      | 0.0000 | 2.0000e-005 | 0.0000 |               | 0.0000       | 0.0000     |                | 0.0000        | 0.0000      | 0.0000   | 4.0000e-005 | 4.0000e-005 | 0.0000 | 0.0000 | 4.0000e-005 |

|       |             |        |             |        |  |        |        |  |        |        |        |             |             |        |        |             |
|-------|-------------|--------|-------------|--------|--|--------|--------|--|--------|--------|--------|-------------|-------------|--------|--------|-------------|
| Total | 8.6200e-003 | 0.0000 | 2.0000e-005 | 0.0000 |  | 0.0000 | 0.0000 |  | 0.0000 | 0.0000 | 0.0000 | 4.0000e-005 | 4.0000e-005 | 0.0000 | 0.0000 | 4.0000e-005 |
|-------|-------------|--------|-------------|--------|--|--------|--------|--|--------|--------|--------|-------------|-------------|--------|--------|-------------|

## 7.0 Water Detail

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### 7.1 Mitigation Measures Water

Install Low Flow Bathroom Faucet

Install Low Flow Kitchen Faucet

Install Low Flow Toilet

Turf Reduction

|          |         | Total CO2 | CH4 | N2O | CO2e |
|----------|---------|-----------|-----|-----|------|
| Category | tons/yr | MT/yr     |     |     |      |

|             |  |        |        |             |        |
|-------------|--|--------|--------|-------------|--------|
| Mitigated   |  | 0.8914 | 0.0167 | 4.0000e-004 | 1.3668 |
| Unmitigated |  | 1.1714 | 0.0198 | 4.8000e-004 | 1.7355 |

## 7.2 Water by Land Use

### Unmitigated

|                                 | Indoor/Outdoor Use  |         | Total CO2     | CH4           | N2O                | CO2e          |
|---------------------------------|---------------------|---------|---------------|---------------|--------------------|---------------|
| Land Use                        | Mgal                | tons/yr | MT/yr         |               |                    |               |
| Fast Food Restaurant with Drive | 0.607067 / 0.038749 |         | 1.1714        | 0.0198        | 4.8000e-004        | 1.7355        |
| <b>Total</b>                    |                     |         | <b>1.1714</b> | <b>0.0198</b> | <b>4.8000e-004</b> | <b>1.7355</b> |

**Mitigated**

|                                 | Indoor/Outdoor Use |         | Total CO2     | CH4           | N2O                | CO2e          |
|---------------------------------|--------------------|---------|---------------|---------------|--------------------|---------------|
| Land Use                        | Mgal               | tons/yr | MT/yr         |               |                    |               |
| Fast Food Restaurant with Drive | 0.512365 / -       |         | 0.8914        | 0.0167        | 4.0000e-004        | 1.3668        |
| <b>Total</b>                    |                    |         | <b>0.8914</b> | <b>0.0167</b> | <b>4.0000e-004</b> | <b>1.3668</b> |

**8.0 Waste Detail**

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**8.1 Mitigation Measures Waste**

Institute Recycling and Composting Services

**Category/Year**

|             |         | Total CO2 | CH4    | N2O    | CO2e    |
|-------------|---------|-----------|--------|--------|---------|
|             | tons/yr | MT/yr     |        |        |         |
| Mitigated   |         | 2.3385    | 0.1382 | 0.0000 | 5.2406  |
| Unmitigated |         | 4.6769    | 0.2764 | 0.0000 | 10.4813 |

## 8.2 Waste by Land Use

### Unmitigated

|                                      | Waste Disposed |         | Total CO2     | CH4           | N2O           | CO2e           |
|--------------------------------------|----------------|---------|---------------|---------------|---------------|----------------|
| Land Use                             | tons           | tons/yr | MT/yr         |               |               |                |
| Fast Food Restaurant with Drive Thru | 23.04          |         | 4.6769        | 0.2764        | 0.0000        | 10.4813        |
| <b>Total</b>                         |                |         | <b>4.6769</b> | <b>0.2764</b> | <b>0.0000</b> | <b>10.4813</b> |

**Mitigated**

|                                      | Waste Disposed |         | Total CO2     | CH4           | N2O           | CO2e          |
|--------------------------------------|----------------|---------|---------------|---------------|---------------|---------------|
| Land Use                             | tons           | tons/yr | MT/yr         |               |               |               |
| Fast Food Restaurant with Drive Thru | 11.52          |         | 2.3385        | 0.1382        | 0.0000        | 5.2406        |
| <b>Total</b>                         |                |         | <b>2.3385</b> | <b>0.1382</b> | <b>0.0000</b> | <b>5.2406</b> |

**9.0 Operational Offroad**

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| Equipment Type | Number | Hours/Day | Days/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|-----------|-------------|-------------|-----------|
|----------------|--------|-----------|-----------|-------------|-------------|-----------|

**10.0 Vegetation**

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**DEPARTMENT OF TRANSPORTATION****DISTRICT 6**

1352 WEST OLIVE AVENUE  
P.O. BOX 12616  
FRESNO, CA 93778-2616  
PHONE (559) 488-7396  
FAX (559) 488-4088  
TTY 711  
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*Serious drought.  
Help save water!*

February 4, 2015

6-TUL-65-29.53 +/-  
2135-IGR/CEQA  
FAST FOOD RESTAURANT  
PRELIMINARY REVIEW

Mr. William Zigler  
Director - Planning & Economic Development  
City of Lindsay  
251 East Honolulu Street  
Post Office Box 369  
Lindsay, CA 93247

Dear Mr. Zigler:

Thank you for directing the property owner's architect to consult with Caltrans to complete a preliminary review of a proposed new 2,000 square foot fast food restaurant with drive-thru (ie: Subway) located at on the northeast corner of the State Route (SR) 65/Hermosa Street intersection. Caltrans has the following comments:

1. Based on the information and preliminary site plan (dated 12-23-14) provided per a meeting with the property owner and the architect (Imelda Golik Inc.) on January 26, 2015, Caltrans estimates that the proposed project would generate approximately 50 trips during the weekday PM peak hour period (per the ITE Trip Generation Manual, 9th Edition). Caltrans anticipates that 90 percent of these trips would have a direct impact to the SR 65/ Hermosa Street intersection.
2. A traffic impact study is not required by Caltrans at this time. Since the intersection is currently signalized with all turn lanes present and currently operating at an acceptable level of service (LOS), additional mitigation is not required at this time.
3. However due to the continuing development within the vicinity, it is anticipated that the operations of the intersection will fall to an unacceptable LOS in the near future. It is recommended that the project proponent contribute towards the City's development impact fee program to fund future improvements to local and State transportation facilities in this area.
4. No new direct access from the proposed development to SR 65 shall be allowed. This segment of SR 65 is classified as an expressway. It is critically important to Caltrans to maintain access control along the mainline of SR 65.

5. If a driveway is to be shared by two or more property owners, an access easement (or an agreement acceptable to the State) needs to be executed between the parties and submitted to the Encroachment Permit office before a permit is issued for any work in the State right of way.
6. Any existing or proposed driveways accessing State right-of-way must meet current State standards.
7. Caltrans recommends that a minimum six feet wide sidewalk be installed along the SR 65 to provide access for pedestrians and be constructed to meet current ADA standards or other applicable State or Federal accessibility and safety requirements.
8. A sidewalk and landscape maintenance agreement will be required.
9. An encroachment permit must be obtained for all proposed activities for placement of encroachments within, under or over the State highway rights-of-way. Activity and work planned in the State right-of-way shall be performed to State standards and specifications, at no cost to the State. Engineering plans, calculations, specifications, and reports (documents) shall be stamped and signed by a licensed Engineer or Architect. Engineering documents for encroachment permit activity and work in the State right-of-way may be submitted using English Units. The Permit Department and the Environmental Planning Branch will review and approve the activity and work in the State right-of-way before an encroachment permit is issued. Encroachment permits will be issued in accordance with Streets and Highway Codes, Section 671.5, "Time Limitations." Encroachment permits do not run with the land. A change of ownership requires a new permit application. Only the legal property owner or his/her authorized agent can pursue obtaining an encroachment permit. Please call the Caltrans Encroachment Permit Office - District 6: 1352 W. Olive, Fresno, CA 93778, at (559) 488-4058.
10. Advertising signs within the immediate area outside the State right-of-way need to be cleared through the Caltrans Division of Traffic Operations, Office of Outdoor Advertising. The project proponent must construct and maintain the advertising signs without access to the State Routes. Please contact the Outdoor Advertising Program, P.O. Box 942874, MS-36, Sacramento, CA 94274-0001, Phone (916) 654-6473, FAX (916) 651-9359 for additional information or to obtain a sign permit application. Additional information on Caltrans Outdoor Advertising Permit requirements may also be found on the Internet at [www.dot.ca.gov/hq/oda](http://www.dot.ca.gov/hq/oda)
11. Any proposed landscaping plans shall meet current standards as determined by the District Landscape Architect. Proposed landscaping needs to be low growing, less than two feet in height, due to sight distance concerns. All features of landscaping shall be evaluated for type, location and site visibility conflicts during the encroachment review process. All permits for landscaping in conventional highway right-of-way must be accompanied by a "District" approved maintenance agreement obligating a local agency or the permittee to maintaining the landscaping. Said maintenance agreement must accompany and be approved

Mr. William Zigler  
February 4, 2015  
Page 3 of 3

prior to issuance of the landscape permit. Proposed landscape projects in access control rights-of-way require an exception process, and approval is subject to the Headquarters Departmental approval process.

12. Dust control measures shall be implemented on the site in a manner to prevent dust from entering the State right-of-way.
13. No water from the proposed project shall flow into the State right-of-way without approval from the District Hydraulic Engineer.
14. As you are aware, Caltrans is currently working on a re-alignment project for SR 65. The project is currently in the Project Approval & Environmental Document Phase. The proposed re-alignment is to the west of the current SR 65. An alternative design that is being reviewed proposes the realignment beginning at Lindmore Street, extending north and ending at Avenue 300, just north of the SR 65/SR 198 interchange. Once this project is completed the current SR 65 right-of-way will be relinquished to the City.

If you have any other questions, please call me at (559) 488-7396.

Sincerely,



DAVID DEEL  
Associate Transportation Planner  
North Planning Branch

Copy via Email: Imelda Golik – Imelda Golik Inc.

## PUBLIC HEARING PROCEDURES

The following rules shall apply:

1. OPEN the public hearing.
2. PROPOSERS (those in favor). The Council may ask questions of the proponents and they may respond.
3. OPPOSERS (those against). The Council may ask questions of the opponents and they may respond.
4. REBUTTAL each side.
5. FURTHER QUESTIONS from Council, but the parties may not engage in further debate.
6. CLOSE the public hearing.
7. COUNCIL DISCUSSION.
8. MOTION ( if necessary).
9. COUNCIL VOTE.



DATE : July 14, 2015  
 TO : Honorable Mayor Padilla and City Council Members  
 FROM : Mike Camarena, City Services Director  
 RE : 2015-2016 Landscape & Lighting Assessment Districts (LLADs) Renewal

Annually, a process of evaluation maintenance needs and establishing an assessment for each LLAD must be followed. At the June 23, 2015 meeting, the City Council preliminarily approved the annual Engineer's Report, which indicates the proposed assessments for the 2015-2016 fiscal year. The assessments, once approved by the City Council, are placed upon the tax bill of property owners as a special assessment, and collected by the County of Tulare.

Summary of Charges per District:

|  |   | 2014-2015 FY |                     | Proposed 2015-2016 FY |                     |                      |                             |
|--|---|--------------|---------------------|-----------------------|---------------------|----------------------|-----------------------------|
|  |   | No. Lots     | Amount per Year/lot | Amount per Month/lot  | Amount per Year/lot | Amount per Month/lot | Increase/Decrease per month |
| <b>Landscape &amp; Lighting Assessment Districts</b> |   |              |                     |                       |                     |                      |                             |
| 1  | Sierra View Estates Assessment District 92-01         | 92           | \$ 203.70           | \$ 16.98              | \$ 165.16           | \$ 13.76             | \$ (3.21)                   |
| 2  | Heritage Park Assessment District 96-01               | 37           | \$ 249.90           | \$ 20.83              | \$ 268.10           | \$ 22.34             | \$ 1.52                     |
| 3  | Parkside Estates Assessment District 01-01            | 44           | \$ 220.44           | \$ 18.37              | \$ 229.92           | \$ 19.16             | \$ 0.79                     |
| 4  | Sweet Brier Plaza (Samoa) Assessment District 02-01   | 6            | \$ 883.34           | \$ 73.61              | \$ 775.24           | \$ 64.60             | \$ (9.01)                   |
| 5  | Sweet Brier Plaza (Hermosa) Assessment District 02-02 | 12           | \$ 735.90           | \$ 61.33              | \$ 623.77           | \$ 51.98             | \$ (9.34)                   |
| 6  | Sierra Vista Assessment District 07-01                | 19           | \$ 10.00            | \$ 0.83               | \$ 56.16            | \$ 4.68              | \$ 3.85                     |
| 7  | Maple Valley Assessment District 07-02                | 42           | \$ 42.70            | \$ 3.56               | \$ 44.64            | \$ 3.72              | \$ 0.16                     |
| 8  | Pelous Ranch Assessment District 09-01                | 105          | \$ 150.88           | \$ 12.57              | \$ 163.24           | \$ 13.60             | \$ 1.03                     |

**Recommendation:**

At this time, staff respectfully requests the City Council to:

Conduct a public hearing on the 2015-2016 Assessments for the Landscape and Lighting Maintenance Districts to take comments or receive protests on the proposed assessments; and

Options:

1. Adopt Resolution No 15-37 ordering the continued maintenance of Landscape and Lighting Maintenance Districts and confirming the Engineer's Report and Assessment for the 2015-2016 fiscal year.
2. Not approve option 1 and provide direction to Staff.

**Attachments:**

Engineer's Reports

Draft Resolution

**RESOLUTION NO. 15-37**  
**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LINDSAY,**  
**ORDERING THE CONTINUED MAINTENANCE OF LANDSCAPING AND**  
**LIGHTING MAINTENANCE DISTRICTS AND CONFIRMING THE**  
**ENGINEER'S REPORT AND ASSESSMENT FOR THE 2015-2016 FISCAL**  
**YEAR.**

At a regularly scheduled meeting of the City Council of the City of Lindsay, held on the 14<sup>th</sup> day of July 2015, at 6:00 p.m. in the Council Chambers of City Hall, Lindsay California 93247, the following resolution was adopted:

**WHEREAS**, the City Council of the City of Lindsay did on the 23<sup>th</sup> day of June 2015, adopt its Resolution of Intention No. 15-33 to order the therein described work in the connection with the continuation of assessment procedures in Landscape and Lighting Maintenance Districts, which Resolution on Intention No.15-33 was duly and legally published in the time, form and manner as required by Law, shown by the Affidavit of Publication of said Resolution of Intention on file in the Office of the City Clerk; and

**WHEREAS**, said City Council having duly received and considered evidence, oral and documentary, concerning the jurisdiction facts in the proceeding and concerning the necessity for the contemplated work and the benefits to be deprived there from, and said City Council having now acquired jurisdiction to order the proposed work;

**NOW, THEREFORE BE IT RESOLVED by the City Council of the City of Lindsay as follows:**

**SECTION 1.** IT IS HEREBY RESOLVED by the City Council of the City of Lindsay that the public interest and convenience require the continuation of assessment procedures for the districts, and said City Council hereby orders that the work and assessment, as set forth and described in said Resolution of Intention No. 15-33 by done and made; and

**SECTION 2.** BE IT FURTHER RESOLVED that the report filed by the Engineer is hereby finally approved; and

**SECTION 3.** BE IT FURTHER RESOLVED that the assessments for fiscal year 2015-2016 and method assessment in the Engineer's Report are hereby approved; and

**SECTION 4.** BE IT FINALLY RESOLVED that the assessments are in compliance with California Code, that they are without regard to property valuation, and in compliance with Chapter 1, Article 4, and Chapter 3, Division 15 of the Streets and Highway Code allowing the placement of the charge on the tax roll for collection.

**PASSED AND ADOPTED** by the City Council of the City of Lindsay this 14<sup>th</sup> day of July, 2015.

ATTEST:

CITY COUNCIL OF THE CITY OF LINDSAY

\_\_\_\_\_  
Carmela Wilson, City Clerk

\_\_\_\_\_  
Ramona Villarreal-Padilla, Mayor

**CITY OF LINDSAY  
STAFF REPORT  
SITE PLAN REVIEW No. 15-05  
(Lindsay Village Apartments)  
July 14, 2015**

---

**GENERAL INFORMATION**

1. Applicant: Self-Help Enterprises  
8445 W. Elowin Ct. / P.O. Box 6520  
Visalia, CA 93290
2. Requested Action: Site Plan Review approval to develop 50 apartment units.
3. Location: The southeast corner of Westwood Avenue and Hermosa Street (APN: 205-040-005).

**PROJECT DESCRIPTION**

Site Plan Review No. 15-05 (Lindsay Village Apartments) is a request by Self-Help Enterprises for site plan review approval to develop 50 apartment units on 2.85 acres of land. The project site is located on the southeast corner of Westwood Avenue and Hermosa Street (APN: 205-040-005).

*Apartments:* The project would consist of 9 apartment buildings that are two-story structures. The 9 apartment buildings will provide 49 affordable rental units and 1 manager's unit. The project would include an additional single-story building, which would provide a manager's office, a maintenance/storage room, a community room, a full kitchen and laundry facilities for a total area of 2,289.5 square feet. Outdoor amenities would include two barbeque areas along with a large grass-based play area, one equipment-based play area and a community garden with planters. The square footage provided within the proposed individual apartment units for each type of building is as follows:

- Building Type A (Tuck-Under Parking):
  - 1 bedroom (625 square feet): 7 units
  - 1 bedroom (650 square feet): 7 units
  - 2 bedroom (814 square feet): 14 units
  - 3 bedroom (1,062 square feet): 14 units
- Building Type B (Townhouse Style):
  - 2 bedroom (720 square feet): 4 units
  - 3 bedroom, townhouse style (1,346 square feet): 4 units

*Energy Efficiency:* The Lindsay Village project will exceed the Title 24 energy code by 15% and includes 100% solar PV to offset the common area and resident loads. Utilization of solar PV to offset residential electricity use can provide meaningful savings on the resident's electricity bills, which helps keep housing affordable and provides more disposable income to spend in the local community. In addition, the solar PV system lowers the overall operating costs of the project, which assists with the long-term viability of the project and frees resources for essential operating functions such as resident services and maintenance.

*Water Efficiency:* In response to the ongoing drought concerns, this project will incorporate a variety of water conservation measures. As a minimum, this project will satisfy all requirements of the California Model Landscape Ordinance and Emergency Water Regulations.

In an effort to exceed these minimum requirements, the Lindsay Village project will also be enrolled in the GreenPoint Rated program and will comply with State CalGreen code. The following list includes some of the water conservation measures that will be incorporated into the project:

- Greywater Recycling System – uses waste water (laundry) for landscape irrigation on as much of the project as possible;
- Comply with Emergency Water Regulations – require water budgets for outdoor irrigation for new residential construction, including all drip irrigation, minimal amounts of turf and mostly low water use shrubs and trees;
- Water Efficient Landscape Measures – Plants Grouped by Water Needs (Hydrozoning); Three Inches of Mulch in Planting Beds; Drought Tolerant, California Native, Mediterranean Species, or Other Appropriate Species; No Turf on Slopes Exceeding 10% and No Overhead Sprinklers Installed in Areas Less Than Eight Feet Wide; Trees to Moderate Building Temperature; High-Efficiency Irrigation System that include moisture sensors to monitor water needs;
- Comply with California Model Landscape Ordinance – requires that any additional standards not met by the items listed above are fully satisfied to ensure water efficiency is maximized;
- Interior Measures – Installation of low-flow faucets, low-flow shower heads, and water efficient toilets.

*Bioswales:* The Lindsay Village project incorporates bioswales along North Westwood Avenue. Bioswales are landscaped/mulched channels that provide treatment, retention and infiltration as they move stormwater from one place to another. Low-water use vegetation in the swales slow, infiltrate, and filter stormwater flows. As linear features, vegetated swales are particularly suitable along streets and parking lots. The Lindsay Village bioswales provide the opportunity to allow excess water to infiltrate and recharge

the aquifer, which is extremely important during the drought and for future water conservation. The bioswales also reduce the amount of water flowing into the storm drain system.

*On-Site Services:* This project includes a variety of on-site resident services. The community room includes space for meetings, educational programs and a computer lab. Services include an after-school program for youth which is provided daily and includes homework assistance and other enrichment activities. Adult education programs include English as a Second Language (ESL), financial planning and management workshops, and healthy living activities such as exercise and cooking classes. Self-Help Enterprises has collaborative partnerships with a variety of community partners, including the Family Health Care Network (reduced fee medical services), CSET and Proteus (workforce development). Self-Help Enterprises also works with community partners to offer additional on-site services such as job training opportunities, resume development, health screenings and other types of on-site services. Overall, the Lindsay Village project includes an extensive program of on-site services which are structured to enrich and enhance the lives of low-income residents.

*Community Garden:* The Lindsay Village project includes a centrally-located community garden. Currently, Self-Help Enterprises operates 1,311 units of affordable housing, including two projects in Goshen. A robust program of resident services, including healthy living classes such as cooking and Zumba are offered in Goshen. Residents that are engaged in the healthy living program have requested a designated space for a community garden, and are working to organize the garden. Since Tulare County suffers from a higher-than-average rate of obesity (the rate of obesity in Tulare County is 43.8% compared to 38% statewide), Self-Help Enterprises wants to offer the opportunity for residents at Lindsay Village to be engaged in healthy living, including active recreation on-site and growing fruits/vegetables in the community garden. Based on the request of residents, community gardens are planned to be incorporated in Goshen and Visalia rental communities, and will incorporate them in future rental communities including Lindsay Village. In the event the community garden is underutilized, the space will be converted to additional recreation space, including permeable pavers, BBQ areas and benches.

*Transportation Improvements:* In addition to the water conservation measures, solar PV and on-site amenities, the Lindsay Village project also includes pedestrian improvements to the corner of Westwood and Hermosa, to assist families in accessing Jefferson Elementary School. Self-Help Enterprises will work with City staff to finalize a set of improvements that improve the safety for walking/biking at the corner of Westwood and Hermosa, and improvements will likely include raised pedestrian crossings and signage that directs pedestrian movement.

## SITE BACKGROUND

The project site is currently used as a small orange grove; this has been the historical use of this site and was originally a part of a two parcel estate. The site is generally flat and has no notable natural or man-made features. To the north of the site are existing single-family homes, to the east is a mobile home complex, bordering south of the project site is agricultural use and the vacant home of the aforementioned estate, and to the west of the site is an apartment complex. It is also important to note that Jefferson Elementary School is located north-west of the site.

## COMPATIBILITY WITH EXISTING ZONING, PLANS AND POLICIES

**Zoning, Land Use:** The Lindsay General Plan designates the project site for medium density residential development (“Medium Density”), and the site is zoned RM-3 (multi-family residential), consistent with the General Plan land use designation. One-family and multi-family dwellings are permitted uses in this zoning district. Multi-family land uses require Council approval through the site plan review process. The Zoning Ordinance includes certain key standards for multi-family uses which should be noted:

- Minimum site area per unit: 3,000 square feet -- *The project would result in approximately one unit per 2,413 square feet of site area; however the State of California requires local agencies to permit a 35% density bonus for low income housing projects. This requirement alters the standard to 1 unit per 1,950 square feet; which the project would satisfy.*
- Maximum site coverage: 50% of the site area may be covered by structures -- *project buildings would cover approximately 24% of the site area, which is well within the standard.*
- Minimum yard requirements:
  - Front: 15 feet *(a minimum of 15 feet is proposed)*
  - Rear: 10 feet for a single story structure, with an additional five feet of yard area for each additional story *(a minimum of 15 feet is proposed)*
  - Side: 5 feet for a single story structure, with an additional five feet of yard area for each additional story *(a minimum of 10 feet is proposed)*
- Minimum distance between structures: 10 feet *(a minimum of 10 feet is proposed)*
- Maximum building height: 35 feet *(32 feet is proposed)*
- Parking: a minimum of 1.5 spaces per 1 bedroom unit and 2 spaces per unit that equals or exceeds 2 bedrooms, per 18.13.020.D.1.b (Parking space schedule for multi-family dwellings). For this project a total of 93 parking spaces are required with a minimum of 4 of these spaces meeting ADA requirements. *(93 spaces are proposed, 7 of which meet ADA requirements)*

Zoning Ordinance Section 18.18.050 lists several findings that are needed for approving Site Plan Review applications. In sum, this includes findings that the project complies with all applicable codes and would not adversely impact public health, safety, or welfare. Staff believes that the project meets these findings, subject to the recommended conditions of approval.

**Circulation:** The site would include access points from both Hermosa Street and Westwood Avenue. Each of these access points are located a minimum of 150 feet from the intersection of Westwood and Hermosa. Internal circulation allows for two-way traffic to navigate the site.

This project will provide 93 parking spaces on site. Seven parking spaces are provided for handicap accessibility and include dedicated adjacent loading zones. Thirty-six spaces are covered by solar carports and thirty-five spaces are covered by “tuck-under parking”. The remaining fifteen spaces are uncovered.

**Public Services:** The site is within a convenient response time of public safety services. Existing and proposed water supply and conveyance facilities would provide adequate fire suppression capabilities. The project would require the installation and extension of utilities to serve the site, as follows:

- **Sewer:** Eight-inch and sixteen inch city sewer lines are available to serve the project site. The project would include extensions of sewer lines and laterals to serve the proposed project.
- **Water:** Eight-inch and ten inch city water lines are available to serve the project site. The project would include extensions of necessary lines to serve the proposed project.
- **Storm Drainage:** twenty-four-inch and forty-eight-inch city storm water drainage lines are available to adequately serve the project site. As a condition of approval, staff is recommending that the developer submit a detailed storm water drainage master plan to accommodate projected storm water impacts of the proposed development.
- **Trash and Recycling:** City trash and recycling services are available to the project site. The project would include trash and recycling enclosures that adequately meet city regulations and service the site.

## PROJECT EVALUATION

Staff finds that the proposed site plan is consistent with the requirements of the Zoning Ordinance. Staff recommends that the project be developed in compliance with specific conditions of approval and development requirements discussed in the following sections.

**Street Improvements:** Staff recommends that adjacent to the project site Hermosa Street would be constructed to City standards, to include an 8 foot wide adjacent sidewalk with tree wells. A minimum of 5 foot wide adjacent sidewalk with tree wells is recommended for Westwood Avenue. Staff recommends that one palm tree, identical in size and maturity to that what exists along Hermosa Street, would be installed along that frontage to continue the corridor appearance. Staff has pointed out to the applicant the possibility of using one of the existing palm trees on site to satisfy this requirement. Furthermore staff has also pointed out the possibility of re-locating multiple existing palm trees along Westwood or within the site.

**Pedestrian Improvements:** Staff recommends that the applicant and the City create a partnership to install pedestrian improvements to the intersection of Westwood and Hermosa to improve safety for students and members of the community. Staff recommends the installation of a raised pedestrian crossing and bulb-outs; however staff recognizes that there may be other options that are more practical and desires to leave the option open for additional analysis. The costs for these improvements are recommended by staff to be shared equally between the applicant and the City.

**Infrastructure:** The attached resolution includes specific recommended conditions of approval for the extent, nature, and timing of required infrastructure improvements (such as sewer, water, storm water drainage, etc.). These conditions would insure that the project is developed consistent with the requirements of city development standards.

**Site Area:** Staff recommends that the developer provide and routinely exercise a site management program that ensures the entire site, especially the trash enclosure, is kept neat and free of litter and weeds.

**Fences and Landscaping:** Staff recommends that fencing along the eastern property lines of this project consist of CMU with a graffiti resistant coating at least six feet in height at a minimum. A detailed landscape plan would be submitted to the approval of the City Planner to ensure conformity with the California Model Landscape Ordinance and the State Emergency Water Regulations. In addition, the applicant would provide a detailed greywater recycling plan; this plan would require approval from the Director of City Services.

**Mechanical Equipment and Utilities:** Mechanical equipment and utility meters/valves/panels/devices would be screened to the greatest extent possible, or located in a manner to be harmonious with the landscaping and architecture, to the approval of the City planner.

**Signage:** All signage would be reviewed under separate permit.

## **ENVIRONMENTAL REVIEW**

An Initial Study has been prepared and approved by Council (Resolution 15-07) in accordance with the requirements of the California Environmental Quality Act (CEQA). The initial study concluded that the project would not result in significant impacts to the environment, based on the adoption of several "mitigation measures" which would reduce potential environmental impacts to a less than significant level. These mitigation measures include:

**Aesthetics:** The project would incorporate standard light shielding measures for street light fixtures to mitigate any potential adverse glare impacts.

**Air Quality:** The project would be subject to all applicable mandatory air pollution control measures of the San Joaquin Valley Unified Air Pollution Control District in effect at time of development, including, but not limited to: Regulation VIII (Fugitive PM10 Prohibitions),

Regulation VIII (Rules 8011-8081), Rule 4102 (Nuisance), 4103 (Open Burning), Rule 4601 (Architectural Coatings), Rule 4641 (Cutback, Slow Cure, and Emulsified Asphalt, Paving and Maintenance Operations), Rule 4901 (Wood Burning Fire Places and Wood Burning Heaters), and Rule 9510 (Indirect Source Review). The developer would specifically demonstrate compliance with San Joaquin Valley Air Pollution Control District Rule 9510 (Indirect Source Review), including payment of all applicable fees, prior to the issuance of the first building permit.

**Cultural Resources:** Pursuant to CEQA Guidelines 15064.5 (f), provisions for historical or unique archaeological resources accidentally discovered during construction would be instituted. Therefore, in the event that any prehistoric or historic subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources would be halted and a qualified archaeologist or paleontologist would be contacted to assess the significance of the find. If any find is determined to be significant, project proponents and the qualified archaeologist and/or paleontologist would meet to determine the appropriate avoidance measures or other appropriate mitigation. All significant cultural materials recovered would be subject to scientific analysis, professional museum curation, and a report prepared by the qualified archaeologist according to current professional standards. If the discovery includes human remains, CEQA Guidelines 15064.5 (e)(1) would be followed.

**Hazards:** Self Help Enterprises (SHE) would remove impacted soil on the northern end of the site that has traces of dieldrin that exceed State environmental screening levels. SHE would excavate impacted soils, obtain confirmation samples to confirm that all impacted soil has been removed, and bring in clean fill soil. This measure would be completed prior to initiation of construction.

**Noise:** High noise levels resulting from construction activities would be limited to the hours of 6:00 a.m. to 10:00 p.m., including weekends and holidays.

**Traffic:** There are several options available to improve pedestrian safety at the corner of Westwood and Hermosa. Self Help Enterprises, in conjunction and cooperation with the City of Lindsay would implement some of the following mitigation measures, dependent upon funding and engineering practicality:

- o Construction of a roundabout
- o Installation of vertical deflections, horizontal shifts, roadway narrowings, or center island narrowings
- o Speed humps
- o Raised crosswalks
- o Rumble strips or other surface treatments
- o Flashing pedestrian crossings
- o Warning signs and/or signage that directs pedestrian movement
- o Temporary in-street signage that can be easily moved by crossing guards or adult volunteers after high traffic arrivals and drop-off periods
- o Providing student/parent education pertaining to safe pedestrian crossing

These mitigation measures are adopted as part of the conditions of approval outlined in Resolution 15-07.

### **RECOMMENDATION**

Staff recommends that the City Council approve Site Plan Review No. 15-05, based on the findings and subject to the conditions found in the attached draft resolution.

### **ATTACHMENTS**

- Draft Resolution 15-36
- Site Plan
- Zoning Map
- Pedestrian Improvement Example
- Resolution 15-07

**RESOLUTION NO. 15-36**

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LINDSAY APPROVING SITE PLAN REVIEW NO. 15-05, A REQUEST BY SELF-HELP ENTERPRISES (8445 W. ELOWIN CT. /P.O. BOX 6520 VISALIA, CA 93290), FOR DEVELOPMENT OF 50 APARTMENT UNITS, FOR PROPERTY LOCATED ON THE SOUTHEAST CORNER OF WESTWOOD AVENUE AND HERMOSA STREET. (APN: 205-040-005).**

At a regularly scheduled meeting of the City Council of the City of Lindsay, held July 14, 2015, at the hour of 6:00 p.m. in the Council Chambers at City Hall, Lindsay, California 93247, the following resolution was adopted:

**THAT WHEREAS**, Site Plan Review No. 15-05 was filed pursuant to the regulations contained in Ordinance No. 437, the Zoning Ordinance of the City of Lindsay; and

**WHEREAS**, planning staff has prepared necessary investigations and prepared a staff report of information bearing upon the site plan review; and

**WHEREAS**, an Initial Study was prepared consistent with the provisions of the California Environmental Quality Act. On the basis of the Initial Study, the City of Lindsay has determined that the project would not result in significant impacts to the environment and has prepared a Mitigated Negative Declaration; and

**WHEREAS**, The Mitigated Negative Declaration was approved on March 10, 2015 by City Council of the City of Lindsay Resolution 15-07.

**NOW, THEREFORE, BE IT RESOLVED**, that the City Council of the City of Lindsay finds that the proposed site plan review is consistent with the provisions of the City of Lindsay Zoning Ordinance (Municipal Code Title 18).

**BE IT FURTHER RESOLVED**, that that the City Council of the City of Lindsay hereby approves Site Plan Review No. 15-05, subject to the following conditions:

**SECTION 1.** That the final plan of development shall be in substantial compliance with the approved site plan, and all applicable standards of Lindsay Development Standards. The final plan of development shall be developed in compliance with the specific conditions listed in the following sections.

**SECTION 2.** That the developer shall comply with the National Pollutant Discharge Elimination System (NPDES) General Permit No. CAS000002 for discharge of Storm Water associated with construction activity (except operations that result in disturbance of less than five acres of total land area and which are not a part of a larger common plan of development or sale). Before construction begins, the proponent must submit a Notice of Intent (NOI) to comply with the permit, a site map, and appropriate fee to the State Water Resources Control Board (SWRCB). The proponent must also prepare a Storm Water Pollution Prevention Plan (SWPPP) for the entire project before construction begins. The SWPPP must contain at a minimum all items listed in Section A of the permit, including descriptions of measures to be taken to prevent or eliminate unauthorized non-storm water discharges and both temporary (e.g., fiber rolls, silt fences, etc.) and permanent (e.g., vegetated swales, detention basins, etc.) best management practices that will be implemented to prevent pollutants from discharging with storm water into water of the United States. If portions of the project are to be sold off before the entire project is completed, the proponent must submit to the California Regional Water Quality Control Board a change of information form identifying the new owners along with a revised site map clearly depicting those portions that were sold and those that are remaining. The proponent is also responsible for

practices that will be implemented to prevent pollutants from discharging with storm water into water of the United States. If portions of the project are to be sold off before the entire project is completed, the proponent must submit to the California Regional Water Quality Control Board a change of information form identifying the new owners along with a revised site map clearly depicting those portions that were sold and those that are remaining. The proponent is also responsible for informing each new owner of their responsibility to submit their own NOI, site map, and appropriate fee to the SWRCB and to prepare their own SWPPP.

**SECTION 3.** That the final plan of development shall provide and show all required utility easements.

**SECTION 4.** That the developer shall remove, under City inspection, all existing, abandoned and unnecessary items from the project site such as foundations, septic tanks, etc., to the satisfaction of the City engineer prior to the acceptance of the site improvements.

**SECTION 5.** That the developer shall abandon and cap existing wells, if any, prior to the start of grading. A well abandonment permit shall be obtained from the Tulare County Department of Environmental Health. Prior to acceptance of the improvements the developer shall provide proof of abandonment in compliance with Tulare County regulations.

**SECTION 6.** That the developer shall assure compliance with all San Joaquin Valley Unified Air Pollution Control District Rules regarding fugitive dust in effect at the time of development, including, but not limited to Rules 8010, 8020, and 8030 as well as Section 7-8, Project Site Maintenance of the Standard Specifications. A street sweeper is to be provided as necessary to comply. During grading operations, the "Supervising Civil Engineer" shall be responsible for enforcing the dust control provisions of Section 7-8 or the developer shall pay inspection fees on the grading cost to compensate the City for dust control inspection. Improvement plans shall show a designated wash out area for concrete trucks, and a sign designating it as such. The developer shall remove and properly dispose of waste concrete deposited in this area.

**SECTION 7.** That this project shall be subject to all applicable mandatory air pollution control measures of the San Joaquin Valley Unified Air Pollution Control District in effect at time of development, including, but not limited to Regulation VIII (Fugitive PM10 Prohibitions), Rules 8011-8081; Rule 4102 (Nuisance); 4103 (Open Burning); Rule 4601 (Architectural Coatings); Rule 4641 (Cutback, Slow Cure, and Emulsified Asphalt, Paving and Maintenance Operations); Rule 4901 (Wood Burning Fire Places and Wood Burning Heaters); Rule 4902 (Residential Water Heaters); Rule 4905 (Natural Gas Fired, Fan-Type Residential Central Furnaces); and Rule 9510 (Indirect Source Review). This mitigation measure shall be monitored by the City of Lindsay through the plan check process for project improvements and building construction.

**SECTION 8.** That the developer shall specifically demonstrate compliance with San Joaquin Valley Air Pollution Control District Rule 9510 (Indirect Source Review), including payment of all applicable fees, prior to the issuance of the first building permit.

**SECTION 9.** That the developer shall provide a Preliminary Soils Report including results on "R-Value" tests and recommendations regarding construction of public improvements, satisfactory to the City engineer, prior to the approval of the improvement plans or start of grading,

whichever comes first.

**SECTION 10.** That any existing above or below ground utility improvements that conflict with the proposed improvements shall be relocated at the developer's expense.

**SECTION 11.** That a lot grading plan shall be included with the improvement plans submitted to the City for approval.

**SECTION 12.** Building or foundation permits for more than one dwelling unit shall not be issued until all of the following items are accepted as complete:

a. The storm drain system is functional so that it will accept water from fire hydrant and/or water main flushing.

b. The water system is functional from the source of water past the lot on which the permit is being requested (i.e., all services and the sampling station, if required, are installed, valves are functional and accessible, bacteria testing is completed, etc.).

c. The site is graded in accordance with the approved grading plan. Prior to receipt of the Final Grading, Drainage and Soils Report, a letter from the "Supervising Civil Engineer" is required validating that the grading has been done in accordance with the approved grading plan and in accordance with the recommendations contained in the Preliminary Soils Report.

e. Site corners are marked.

f. Fire hydrants, as appropriate, are installed and accepted by the Director of Public Safety and the City engineer.

**SECTION 13.** That the adjacent section of Westwood Avenue shall be developed as follows:

a. A minimum of 15 gallon street trees shall be provided adjacent to Westwood Avenue per approved landscape and irrigation plans.

b. A five (5) foot wide street adjacent sidewalk with tree wells shall be provided and shall provide for handicapped returns at crosswalks.

**SECTION 14.** That the adjacent section of Hermosa Street shall be developed as follows:

a. An eight (8) foot wide street adjacent sidewalk with tree wells shall be provided and shall provide for handicapped returns at crosswalks.

b. The existing palm trees fronting on to Hermosa Street shall not be altered.

c. One (1) new mature palm tree identical to what exists is planted approximately fifty feet west of the most westerly existing palm tree to match existing spacing between street fronting palm trees along Hermosa.

**SECTION 15.** That water lines of sufficient size and capacity shall be installed to serve the project by the developer, at his expense. Said design is to be approved by the City engineer, and shall preferably connect with the existing 10" water lines in Westwood Avenue. If it is not feasible to connect water services to Westwood Avenue, then the 8" water lines in Hermosa Street shall be utilized. All water lines shall be inter-looped in accordance with approved improvement plans for circulation with new and existing lines in the area. Improvement plans shall define location and adequate size of facilities to the approval of the City engineer. Said design shall provide for expansion to serve future development, if required by the City engineer. All construction shall be in accordance with the Lindsay Development Standards and Standard Specifications for Public Works Construction (latest approved edition).

**SECTION 16.** That sewer lines of sufficient size and capacity shall be installed to serve the project by the developer, at his expense. Said design is to be approved by the City engineer, and shall preferably connect with the existing 8" sewer lines in Westwood Avenue. If it is not feasible to connect sewer services to Westwood Avenue, then the 16" sewer lines in Hermosa Street shall be utilized. The developer shall be responsible for installing, at his expense; any needed modifications to offsite sewer lift stations necessary to serve the proposed development, if required. Improvement plans shall define location and adequate size of facilities to the approval of the City engineer. Said design shall provide for expansion to serve future development, if required by the City engineer. All sewer lines shall be interconnected with existing lines in the area, in accordance with approved improvement plans. All construction shall be in accordance with the Lindsay Development Standards and Standard Specifications for Public Works Construction (latest approved edition).

**SECTION 17.** That the developer shall provide a master drainage and grading plan for the development in accordance with the City's Master Storm Drain Plan. Provisions for storm drainage of the site shall be provided by the developer to the approval of the City engineer. Said provisions shall control surface flows in accordance with development standards and the City's Master Drainage Plan.

**SECTION 18.** That the developer shall install, at his expense, required City standard fire hydrant assemblies per the approved development plan.

**SECTION 19.** That the developer shall install, at his expense, required 5800 lumen street lights on standard 25 foot marbelite poles in the public right-of-way along Westwood Avenue, as recommended by Southern California Edison and approved by the City engineer per the approved development plan. The developer shall incorporate standard light shielding measures for exterior light fixtures to mitigate any potential adverse glare impacts.

**SECTION 20.** That the developer shall install, at his expense, required 5800 lumen street lights on 25 foot marbelite poles in the public right-of-way that match the design of existing street lights along Hermosa Street, as recommended by Southern California Edison and approved by the City engineer per the approved development plan. The developer shall incorporate standard light shielding measures for exterior light fixtures to mitigate any potential adverse glare impacts.

**SECTION 21.** That the developer shall underground all telephone, power, cable television, and communications utilities and shall provide and convey necessary easements to the applicable utility companies.

**SECTION 22.** That the developer shall coordinate with the U.S. Postal Service regarding mail facilities that will be utilized. Neighborhood Box Units, if required shall be used and installed in accordance with U.S. Postal Service standards.

**SECTION 23.** That the developer shall install trash and recycling enclosures, providing sufficient container capacity to the approval of the City engineer. Said enclosures shall incorporate masonry construction and graffiti resistant coating, shall be ADA accessible, and shall provide visual screening from all public rights-of-way and dwelling units.

**SECTION 24.** That the developer shall install a boundary wall of split faced masonry on the east property line.

**SECTION 25.** That the developer shall provide a landscaping and irrigation plan. This plan shall incorporate water conservation design and materials in accordance with the State Model Landscape Ordinance, and the State Emergency Water Regulations. This plan shall provide for maximum feasible shading of pavement areas.

**SECTION 26.** That the developer shall provide a detailed greywater recycling plan that is to be approved by the Director of City Services.

**SECTION 27.** That landscaping shall provide for low growing plants at intersections, in accordance with City ordinances, where traffic sight distances can become a problem.

**SECTION 28.** That the developer shall construct curb, gutter, sidewalks or pedestrian paths, and street improvements on all interior drives, as appropriate.

**SECTION 29.** That the developer shall provide permanent common open space and recreational areas, consistent with the approved plan of development, and installed concurrently with the development. These facilities shall be completed prior to the issuance of certificates of occupancy for any apartment building on site. It shall be the responsibility of the property owner to assure that these facilities are maintained for ongoing resident use and retained on site permanently. Removal or substantial modification resulting in reduced recreational building or land area shall require an amendment of this site plan review approval.

**SECTION 30.** That the developer shall cover one half of the cost of the installation of pedestrian safety improvements at the intersection of Westwood and Hermosa. Improvements shall include "corner bulb-outs at the west side of the Westwood Avenue / Hermosa Street intersection along with a raised crosswalk across Hermosa Street" or other appropriate safety improvements as approved by the City. In no case shall the one-half cost of pedestrian improvements exceed that of the aforementioned measures shown in quotations. Additionally, the corner pedestrian bulb-out shown on the approved site plan shall be installed at the sole expense of the developer.

**SECTION 31.** That the developer shall create and routinely exercise a site management program that ensures the entire site is kept neat and free of litter and weeds.

**SECTION 32.** That the developer shall screen mechanical equipment and utility meters/valves/panels/devices to the greatest extent possible, or located in a manner to be harmonious with the landscaping and architecture, to the approval of the City planner.

**SECTION 33.** That the developer shall be responsible for cost and related labor involving installation of necessary street signs and regulatory signs in conjunction with this development.

**SECTION 34.** That all signage shall be reviewed under separate permit.

**SECTION 35.** That any developer-owned, undeveloped area that is part of or adjacent to this project shall be routinely treated for weeds and kept neat and free of litter.

**SECTION 36.** That all required fees and sureties (plan checks, inspections, development fees, agreements, etc.), as appropriate shall be paid and/or secured by the developer in the amounts, at the times and in the manner prescribed by municipal codes, ordinances and policies of the City of Lindsay.

**SECTION 37.** That the developer shall pay all applicable fees including, water, sewer, storm drain acreage, connection, park land, etc.

**SECTION 38.** That all other city codes and ordinances shall apply.

**PASSED, APPROVED AND ADOPTED** by the City Council of the City of Lindsay this 14th day of July, 2015.

ATTEST:

CITY COUNCIL OF THE CITY OF LINDSAY

\_\_\_\_\_  
Carmela Wilson, City Clerk

\_\_\_\_\_  
Ramona Villarreal-Padilla, Mayor



**Unit Count**  
 Building Type A (7 Buildings)  
 Tuck-Under Parking  
 6 Units:  
 (2) 1 BR Flats  
 (2) 2 BR Flats  
 (2) 3 BR Flats

Building Type B (1 Building)  
 (4) 3 BR Townhouses  
 (4) 2 BR Flats

**Totals:**  
 (14) 1 BR  
 (18) 2 BR  
 (18) 3 BR  
 50 Units Total

Total Site Area: 2.85 Acres  
 93 Total On-Site Parking Spaces

bbq and picnic area  
 trellis  
 Existing Mobile Homes  
 new 6' tall concrete masonry unit wall  
 bioswale  
 community garden and planters  
 new 6' tall decorative metal fence

Existing Jefferson Elementary School  
 existing palm trees  
 monument sign  
 drought tolerant landscaping per Green Point Rating, Cal Green, and the State of California's emergency outdoor water use regulations effective June 01, 2015  
 bbq  
 play structure  
 trellis over mail area  
 trash and recycling  
 edge of solar carport  
 tuck-under parking  
 Existing Apartments  
 bioswale  
 edge of solar carport  
 trellis

North Westwood Ave

West Hermosa Street

Existing Orange Orchard



# CITY OF LINDSAY ZONING MAP



**Legend**

- City Limits
- Railroad
- State Highway
- Streets
- Parcels

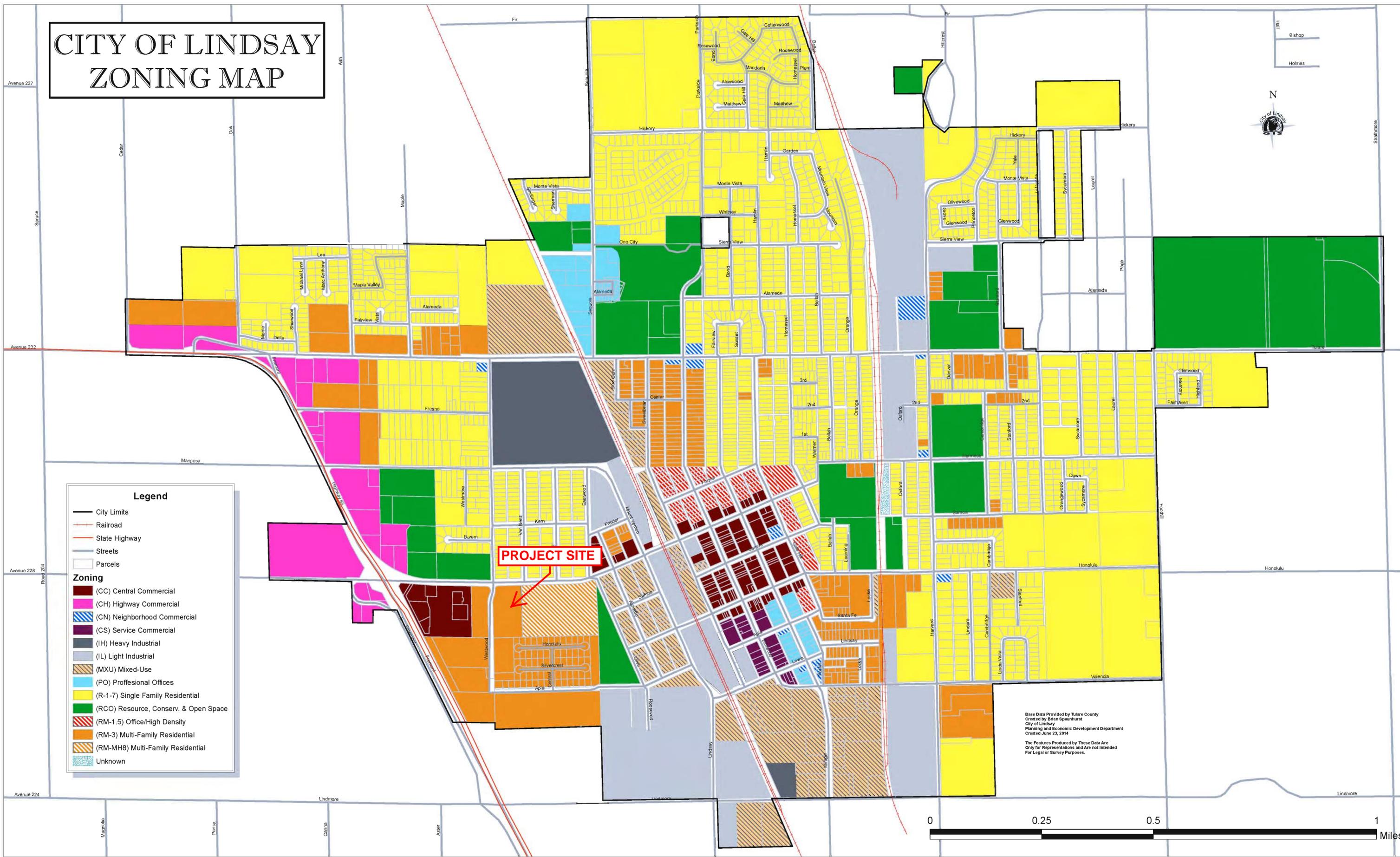
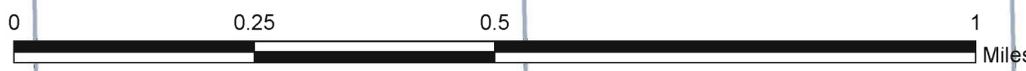
**Zoning**

- (CC) Central Commercial
- (CH) Highway Commercial
- (CN) Neighborhood Commercial
- (CS) Service Commercial
- (IH) Heavy Industrial
- (IL) Light Industrial
- (MXU) Mixed-Use
- (PO) Professional Offices
- (R-1-7) Single Family Residential
- (RCO) Resource, Conserv. & Open Space
- (RM-1.5) Office/High Density
- (RM-3) Multi-Family Residential
- (RM-MH8) Multi-Family Residential
- Unknown

**PROJECT SITE**

Base Data Provided by Tulare County  
 Created by Brian Spaulhurst  
 City of Lindsay  
 Planning and Economic Development Department  
 Created June 23, 2014

The Features Produced by These Data Are  
 Only for Representations and Are not Intended  
 For Legal or Survey Purposes.



**RESOLUTION NO. 15-07**

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LINDSAY APPROVING A MITIGATED NEGATIVE DECLARATION FOR PLANNED UNIT DEVELOPMENT NO. 15-05, A REQUEST BY SELF-HELP ENTERPRISES, FOR PROPERTY LOCATED AT THE SOUTHEAST CORNER OF HERMOSA STREET AND WESTWOOD AVENUE (APN: 205-040-005).**

At a regularly scheduled meeting of the City Council of the City of Lindsay, held March 10, 2015 at the hour of 6:00 p.m. in the Council Chambers at City Hall, Lindsay, California, 93247, the following resolution was adopted:

**THAT WHEREAS**, an environmental Initial Study/Mitigated Negative Declaration for Planned Unit Development No. 15-05 was filed pursuant to the regulations contained in Ordinance No. 437, the Zoning Ordinance of the City of Lindsay; and

**WHEREAS**, Planned Unit Development 15-05 involves the construction of up to sixty (60) affordable housing units and community center by Self-Help Enterprises on a three-acre portion of APN: 205-040-005; and

**WHEREAS**, California Government Code Section 65915 governs both general law and charter cities and requires a 35% density bonus for housing developments providing 20 percent of the total units to low-income residents, or 11 percent of the units to very-low-income residents; and

**WHEREAS**, Self-Help Enterprises intends to provide 100% of its units to low and very-low income residents for the required 55-year minimum timeframe and thus qualifies for the State required maximum density bonus of 35 percent, which would permit 19.6 units per acre for a total of 60 units on the proposed three-acre site; and

**WHEREAS**, the City Council of the City of Lindsay, after twenty (20) days published notice, did hold a public hearing before said Council on March 10, 2015, and

**WHEREAS**, an Initial Study was prepared consistent with the provisions of the California Environmental Quality Act (CEQA). On the basis of the Initial Study, the City of Lindsay has determined that the project would not result in a significant effect on the environment, and has prepared a Mitigated Negative Declaration.

**NOW, THEREFORE, BE IT RESOLVED** that the proposed project would not result in a significant effect on the environment, and the City Council hereby accepts and adopts the Mitigated Negative Declaration, subject to the following mitigation measures:

**SECTION 1. Aesthetics:** The project shall incorporate standard light shielding measures for street light fixtures to mitigate any potential adverse glare impacts.

**SECTION 2. Air Quality:** The project shall be subject to all applicable mandatory air pollution control measures of the San Joaquin Valley Unified Air Pollution Control District in effect at time of development, including, but not limited to: Regulation VIII (Fugitive PM10 Prohibitions), Regulation VIII (Rules 8011-8081), Rule 4102 (Nuisance), 4103 (Open Burning), Rule 4601 (Architectural Coatings), Rule 4641 (Cutback, Slow Cure, and Emulsified Asphalt, Paving and Maintenance Operations), Rule 4901 (Wood Burning Fire Places and Wood Burning Heaters), and Rule 9510 (Indirect Source Review). The developer shall specifically demonstrate compliance with San Joaquin Valley Air Pollution Control District Rule 9510 (Indirect Source Review), including payment of all applicable fees, prior to the issuance of the first building permit.

**SECTION 3. Cultural Resources:** Pursuant to CEQA Guidelines 15064.5 (f), provisions for historical or unique archaeological resources accidentally discovered during construction shall be instituted. Therefore, in the event that any prehistoric or historic subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be

halted and a qualified archaeologist or paleontologist shall be contacted to assess the significance of the find. If any find is determined to be significant, project proponents and the qualified archaeologist and/or paleontologist shall meet to determine the appropriate avoidance measures or other appropriate mitigation. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and a report prepared by the qualified archaeologist according to current professional standards. If the discovery includes human remains, CEQA Guidelines 15064.5 (e)(1) shall be followed.

**SECTION 4. Hazards:** Self Help Enterprises (SHE) shall remove impacted soil on the northern end of the site that has traces of dieldrin that exceed State environmental screening levels. SHE shall excavate impacted soils, obtain confirmation samples to confirm that all impacted soil has been removed, and bring in clean fill soil. This measure would be completed prior to initiation of construction.

**SECTION 5. Noise:** High noise levels resulting from construction activities shall be limited to the hours of 6:00 a.m. to 10:00 p.m., including weekends and holidays.

**SECTION 6. Traffic:** There are several options available to improve pedestrian safety at the corner of Westwood and Hermosa. Self Help Enterprises, in conjunction and cooperation with the City of Lindsay shall implement some of the following mitigation measures, dependent upon funding and engineering practicality:

- Construction of a roundabout
- Installation of vertical deflections, horizontal shifts, roadway narrowings, or center island narrowings
- Speed humps
- Raised crosswalks
- Rumble strips or other surface treatments
- Flashing pedestrian crossings
- Warning signs and/or signage that directs pedestrian movement
- Temporary in-street signage that can be easily moved by crossing guards or adult volunteers after high traffic arrivals and drop-off periods
- Providing student/parent education pertaining to safe pedestrian crossing

**NOW THEREFORE BE IT RESOLVED** that this approval is for the environmental initial study and determination only.

**BE IT FURTHER RESOLVED** that the developer shall submit a separate application and site plan for approval of Planned Unit Development 15-05 where project details shall be reviewed by the Lindsay City Council during a noticed public hearing. At that time the City Council may approve, disapprove, or impose conditions of approval to the proposed project. The payment of any remaining fees shall be required at the time of application.

**PASSED, APPROVED AND ADOPTED** by the City Council of the City of Lindsay this 10th day of March, 2015.

ATTEST:

CITY COUNCIL OF THE CITY OF LINDSAY

  
\_\_\_\_\_  
Carmela Wilson, City Clerk

  
\_\_\_\_\_  
Ramona Villarreal-Padilla, Mayor

CITY OF LINDSAY            )  
COUNTY OF TULARE        )  
STATE OF CALIFORNIA      )     CITY CLERK CERTIFICATION

I, Carmela Wilson, City Clerk for he City of Lindsay, do hereby certify that the foregoing is a full, true and correct copy of the original Resolution No.15-07 adopted by the City Council of the City of Lindsay at a Regular meeting duly held on the 10th day of March, 2015, on motion of SALINAS, second of SANCHEZ, by the following vote, as the same appears of record and on file in my office:

AYES:            SALINAS, SANCHEZ, KIMBALL, PADILLA.  
NOES:            None.  
ABSENT:         MECUM.  
ABSTAIN:        None.

WITNESS my hand and Corporate Seal of Lindsay this 10th of March 2015.

OFFICE OF THE CITY CLERK OF LINDSAY

  
\_\_\_\_\_  
Carmela Wilson, City Clerk